

# Briargate-Stapleton Corridor Study

## Appendix E: Cost Estimates

El Paso County Department of Public Works

On-Call Contract:  
#17-067-51 12/09/2021

**DRAFT**

# Briargate-Stapleton Corridor Study

## Appendix E: Cost Estimates



Prepared for

El Paso County Department of Public Works  
On-Call Contract: #17-067-51

**DRAFT**

December 9, 2021

Prepared by

**WILSON**  
**& COMPANY**

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Colorado Springs, CO 80919  
719-520-5800

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Engineers Opinion of Probable Cost	
Project: Description	<i>Briarcliff Pkwy/Stapleton Dr - Initial</i>
Project Number	<i>20-100-006-00</i>
Name of Preparer	<i>Justine Fedde, PE</i>

Item No.	Item Description	Unit	Unit Cost	Quantity	Cost
202-00240	Remove Asphalt Mat (Planing)	Sq Yard	\$2.60	12,000	\$31,200
203-00060	Embankment Material (CIP)	Cubic Yard	\$17.00	310,200	\$5,273,400
304-06000	ABC (CL 6)	Ton	\$29.00	69,000	\$2,001,000
403-34721	HMA (Grade SX) (75) (PG 58-28)	Ton	\$93.00	51,000	\$4,743,000
606-00301	Guardrail Type 3 (6-3)	Lin Foot	\$37.00	1,500	\$55,500
606-00910	Guardrail Type 9 (Style CA)	Lin Foot	\$110.00	150	\$16,500
608-00000	Concrete Sidewalk	Sq Yard	\$85.00	38,400	\$3,264,000
609-21010	Curb and Gutter Type 2 I-B	Lin Foot	\$36.00	30,300	\$1,090,800
613-10000	Wiring	LUMP SUM	\$75,000.00	2	\$150,000
613-13000	Luminaire (LED) (Special)	Each	\$1,700.00	8	\$13,600
614-70150	Pedestrian Signal Face (16) (Countdown)	Each	\$670.00	16	\$10,720
614-70336	Traffic Signal Face (12-12-12)	Each	\$890.00	30	\$26,700
614-70560	Traffic Signal Face (12-12-12-12-12)	Each	\$1,400.00	10	\$14,000
614-72860	Pedestrian Push Button	Each	\$840.00	16	\$13,440
614-72886	Intersection Detect System (Camera)	Each	\$7,500.00	8	\$60,000
614-81150	Signal-Light Pole Steel (1-50 Ft)	Each	\$21,000.00	8	\$168,000
614-84000	Traffic Signal Pedestrian Pole Steel	Each	\$3,300.00	16	\$52,800
614-86240	Controler (Type 170)	Each	\$7,100.00	2	\$14,200
900-	BRIDGE	SF	\$150.00	5,980	\$897,000
900-	DRAINAGE (from project team estimate)	LUMP SUM	\$8,477,000.00	1	\$8,477,000
=	WALL	SF	\$80.00	11,519	\$921,504

#### NOTES:

The design upon which this opinion of probable cost was based is highly conceptual. As a result, we recommend that a 20% - 30% contingency be used in order to cover additional costs.

#### Assumptions:

-Bridges will be placed under only the portion of the roadway that is currently being constructed.

ITEM COST SUBTOTAL:		\$27,295,000
Contingency:	30%	\$8,189,000.00
ITEM COST WITH CONTINGENCY:		\$35,484,000
Mobilization:	10.00%	\$3,549,000
Utilities	5.00%	\$1,775,000
Right of Way	2.00%	\$710,000
Force Account Provision:	10.00%	\$3,549,000
CONSTRUCTION SUBTOTAL:		\$9,583,000
Engineering and Environmental Fees		
Design Fee:	10.00%	\$3,549,000
Environmental Clearance Fee:	2.00%	\$710,000
Construction Engineering	10.00%	\$3,549,000
FEE SUBTOTAL:		\$7,808,000
		<b>d = a+b+c</b>
TOTAL PROGRAM COST:		\$52,900,000

a

b

c

d

Costs highlighted in black are percentages applied to the Item Cost With Contingency (a)



Engineers Opinion of Probable Cost		
Project: Description	Priarqde Hwy/Stapleton Dr - Interim	
Project Number	20-100-006-00	
Name of Preparer	Justine Fedde, PE	

Item No.	Item Description	Unit	Unit Cost	Quantity	Cost
202-00240	Remove Asphalt Mat (Planing)	Sq Yard	\$2.60	12,000	\$31,200
203-00080	Embankment Material (CIP)	Cubic Yard	\$17.00	495,000	\$8,415,000
304-06000	ABC (CL 6)	Ton	\$29.00	137,500	\$3,987,500
403-34721	HMA (Grade SX) (75) (PG 58-28)	Ton	\$93.00	101,000	\$9,393,000
606-00301	Guardrail Type 3 (6-3)	Lin Foot	\$37.00	5,950	\$220,150
606-00910	Guardrail Type 9 (Style CA)	Lin Foot	\$110.00	600	\$66,000
608-00000	Concrete Sidewalk	Sq Yard	\$85.00	28,800	\$2,448,000
609-21010	Curb and Gutter Type 2 I-B	Lin Foot	\$36.00	60,500	\$2,178,000
609-21020	Curb and Gutter Type 2 II-B	Lin Foot	\$35.00	60,500	\$2,117,500
610-00026	Median Cover (6 In Pattern Concrete)	Sq Foot	\$12.00	64,800	\$777,600
613-10000	Wiring	LUMP SUM	\$75,000.00	2	\$150,000
613-13000	Luminaire (LED) (Special)	Each	\$1,700.00	8	\$13,600
614-70150	Pedestrian Signal Face (16) (Countdown)	Each	\$670.00	16	\$10,720
614-70335	Traffic Signal Face (12-12-12)	Each	\$890.00	30	\$26,700
614-70560	Traffic Signal Face (12-12-12-12)	Each	\$1,400.00	10	\$14,000
614-72860	Pedestrian Push Button	Each	\$840.00	16	\$13,440
614-72886	Intersection Detect System (Camera)	Each	\$7,500.00	8	\$60,000
614-81150	Signal-Light Pole Steel (1-50 Ft)	Each	\$21,000.00	8	\$168,000
614-84000	Traffic Signal Pedestrian Pole Steel	Each	\$3,300.00	16	\$52,800
614-86240	Controler (Type 170)	Each	\$7,100.00	2	\$14,200
900-	BRIDGE	SF	\$150.00	10,235	\$1,535,250
900-	DRAINAGE (from project team estimate)	LUMP SUM	\$13,919,000.00	1	\$13,919,000
900-	WALL	SF	\$80.00	11,519	\$921,504

**NOTES:**  
The design upon which this opinion of probable cost was based is highly conceptual. As a result, we recommend that a 20% 30% contingency be used in order to cover additional costs.

**Assumptions:**  
-Bridges will be placed under only the portion of the roadway that is currently being constructed.

<b>ITEM COST SUBTOTAL:</b>		<b>\$46,534,000</b>
<b>Contingency:</b>	<b>30%</b>	<b>\$13,961,000.00</b>
<b>ITEM COST WITH CONTINGENCY:</b>		<b>\$60,495,000</b>
<b>Mobilization:</b>	10.00%	\$6,050,000
<b>Utilities</b>	5.00%	\$3,025,000
<b>Right of Way</b>	2.00%	\$1,210,000
<b>Force Account Provision:</b>	10.00%	\$6,050,000
<b>CONSTRUCTION SUBTOTAL:</b>		<b>\$16,335,000</b>
<b>Engineering and Environmental Fees</b>		
<b>Design Fee:</b>	10.00%	\$6,050,000
<b>Environmental Clearance Fee:</b>	2.00%	\$1,000
<b>Construction Engineering</b>	10.00%	\$6,050,000
<b>FEE SUBTOTAL:</b>		<b>\$12,101,000</b>
<b>d = a+b+c</b>		
<b>TOTAL PROGRAM COST:</b>		<b>\$88,900,000</b>

a  
b  
c  
d

Costs highlighted in black are percentages applied to the Item Cost With Contingency (a)



Engineers Opinion of Probable Cost	
Project: Description	Priorgate Pkwy/Stapleton Dr - Phased Interim
Project Number	20-100-006-00
Name of Preparer	Justine Fedde, P.E.

Item No.	Item Description	Unit	Unit Cost	Quantity	Cost
203-00060	Embankment Material (CIP)	Cubic Yard	\$17.00	310,200	\$5,273,400
304-06000	ABC (CL 6)	Ton	\$29.00	69,000	\$2,001,000
403-34721	HMA (Grade SX) (75) (PG 58-28)	Ton	\$93.00	51,000	\$4,743,000
606-00301	Guardrail Type 3 (6-3)	Lin Foot	\$37.00	1,500	\$55,500
606-00910	Guardrail Type 9 (Style CA)	Lin Foot	\$110.00	150	\$16,500
609-21010	Curb and Gutter Type 2 I-B	Lin Foot	\$36.00	30,300	\$1,090,800
610-00026	Median Cover (6 In Pattern Concrete)	Sq Foot	\$12.00	64,800	\$777,600
900-	BRIDGE	SF	\$150.00	4,600	\$690,000
900-	DRAINAGE (from project team estimate)	LUMP SUM	\$5,442,000.00	1	\$5,442,000
900-	WALL	SF	\$80.00	11,519	\$921,504

#### NOTES:

The design upon which this opinion of probable cost was based is highly conceptual. As a result, we recommend that a 20% - 30% contingency be used in order to cover additional costs.

#### Assumptions:

-Bridges will be placed under only the portion of the roadway that is currently being constructed.

ITEM COST SUBTOTAL:		\$21,012,000
Contingency:	30%	\$6,304,000.00
ITEM COST WITH CONTINGENCY:		\$27,316,000
Mobilization:	10.00%	\$2,732,000
Utilities	5.00%	\$1,366,000
Right of Way	2.00%	\$547,000
Force Account Provision:	10.00%	\$2,732,000
CONSTRUCTION SUBTOTAL:		\$7,377,000
Engineering and Environmental Fees		
Design Fee:	10.00%	\$2,732,000
Environmental Clearance Fee:	2.00%	\$547,000
Construction Engineering	10.00%	\$2,732,000
FEE SUBTOTAL:		\$6,011,000
		<b>d = a+b+c</b>
TOTAL PROGRAM COST:		\$40,700,000

a

b

c

d

Costs highlighted in black are percentages applied to the Item Cost With Contingency (a)



Engineers Opinion of Probable Cost	
Project: Description	<i>Priargale Pkwy/Stapleton Dr - Phased Ultimate</i>
Project Number	<i>20-100-006-00</i>
Name of Preparer	<i>Justine Fedde, PE</i>

Item No.	Item Description	Unit	Unit Cost	Quantity	Cost
203-00060	Embankment Material (CIP)	Cubic Yard	\$17.00	10,000	\$170,000
603-01300	30 In RCP	Lin Foot	\$250.00	30,525	\$7,631,205
604-19210	Inlet Type R L10 (10 Ft)	Each	\$11,000.00	96	\$1,055,890
606-00301	Guardrail Type 3 (6-3)	Lin Foot	\$37.00	2,880	\$106,549
606-00910	Guardrail Type 9 (Style CA)	Lin Foot	\$110.00	288	\$31,677
608-00000	Concrete Sidewalk	Sq Yard	\$85.00	39,000	\$3,315,000
609-21020	Curb and Gutter Type 2 II-B	Lin Foot	\$35.00	60,500	\$2,117,500
610-00026	Median Cover (6 In Pattern Concrete)	Sq Foot	\$12.00	0	\$0
613-10000	Wiring	LUMP SUM	\$75,000.00	0	\$0
613-13000	Luminaire (LED) (Special)	Each	\$1,700.00	0	\$0
614-70150	Pedestrian Signal Face (16) (Countdown)	Each	\$670.00	0	\$0
614-70336	Traffic Signal Face (12-12-12)	Each	\$890.00	0	\$0
614-70560	Traffic Signal Face (12-12-12-12-12)	Each	\$1,400.00	0	\$0
614-72860	Pedestrian Push Button	Each	\$840.00	0	\$0
614-72886	Intersection Detect System (Camera)	Each	\$7,500.00	0	\$0
614-81150	Signal-Light Pole Steel (1-50 Ft)	Each	\$21,000.00	0	\$0
614-84000	Traffic Signal Pedestrian Pole Steel	Each	\$3,300.00	0	\$0
614-86240	Controler (Type 170)	Each	\$7,100.00	0	\$0
900-	BRIDGE	SF	\$150.00	0	\$0
900-	DRAINAGE (from project team estimate)	LUMP SUM	\$0.00	1	\$0
900-	WALL	SF	\$80.00	0	\$0

**NOTES:**

The design upon which this opinion of probable cost was based is highly conceptual. As a result, we recommend that a 20% - 30% contingency be used in order to cover additional costs.

**Assumptions:**

-Bridges will be constructed in a previous phase.

<b>ITEM COST SUBTOTAL:</b>		<b>\$14,428,000</b>
<b>Contingency:</b>	<b>30%</b>	<b>\$4,329,000.00</b>
<b>ITEM COST WITH CONTINGENCY:</b>		<b>\$18,757,000</b>
Mobilization:	10.00%	\$1,876,000
Utilities	5.00%	\$938,000
Right of Way	2.00%	\$376,000
Force Account Provision:	10.00%	\$1,876,000
<b>CONSTRUCTION SUBTOTAL:</b>		<b>\$5,066,000</b>
<b>Engineering and Environmental Fees</b>		
Design Fee:	10.00%	\$1,876,000
Environmental Clearance Fee:	2.00%	\$376,000
Construction Engineering	10.00%	\$1,876,000
<b>FEE SUBTOTAL:</b>		<b>\$4,128,000</b>
		<b>d = a+b+c</b>
<b>TOTAL PROGRAM COST:</b>		<b>\$28,000,000</b>

a

b

c

d

Costs highlighted in black are percentages applied to the Item Cost With Contingency (a)





Engineers Opinion of Probable Cost	
Project: Description	<i>Priargate Pkwy./Stapleton Dr - Ullinle</i>
Project Number	<i>20-100-006-00</i>
Name of Preparer	<i>Justine Fedde, P.E.</i>

Item No.	Item Description	Unit	Unit Cost	Quantity	Cost
202-00240	Remove Asphalt Mat (Planing)	Sq Yard	\$2.60	54,000	\$140,400
203-00060	Embankment Material (CIP)	Cubic Yard	\$17.00	412,500	\$7,012,500
304-06000	ABC (CL 6)	Ton	\$29.00	107,000	\$3,103,000
403-34721	HMA (Grade SX) (75) (PG 58-28)	Ton	\$93.00	79,000	\$7,347,000
606-00301	Guardrail Type 3 (6-3)	Lin Foot	\$37.00	6,000	\$222,000
606-00910	Guardrail Type 9 (Style CA)	Lin Foot	\$110.00	600	\$66,000
608-00000	Concrete Sidewalk	Sq Yard	\$85.00	57,600	\$4,896,000
609-21010	Curb and Gutter Type 2 I-B	Lin Foot	\$36.00	60,500	\$2,178,000
609-21020	Curb and Gutter Type 2 II-B	Lin Foot	\$35.00	60,500	\$2,117,500
610-00026	Median Cover (6 In Pattern Concrete)	Sq Foot	\$12.00	64,800	\$777,600
613-10000	Wiring	LUMP SUM	\$75,000.00	2	\$150,000
613-13000	Luminaire (LED) (Special)	Each	\$1,700.00	8	\$13,600
614-70150	Pedestrian Signal Face (16) (Countdown)	Each	\$670.00	16	\$10,720
614-70336	Traffic Signal Face (12-12-12)	Each	\$890.00	30	\$26,700
614-70560	Traffic Signal Face (12-12-12-12)	Each	\$1,400.00	10	\$14,000
614-72860	Pedestrian Push Button	Each	\$840.00	16	\$13,440
614-72886	Intersection Detect System (Camera)	Each	\$7,500.00	8	\$60,000
614-81150	Signal-Light Pole Steel (1-50 Ft)	Each	\$21,000.00	8	\$168,000
614-84000	Traffic Signal Pedestrian Pole Steel	Each	\$3,300.00	16	\$52,800
614-86240	Controller (Type 170)	Each	\$7,100.00	2	\$14,200
900-	BRIDGE	SF	\$150.00	7,500	\$1,125,000
900-	DRAINAGE (from project team estimate)	LUMP SUM	\$13,919,000.00	1	\$13,919,000
900-	WALL	SF	\$80.00	12,000	\$960,000

**NOTES:**

The design upon which this opinion of probable cost was based is highly conceptual. As a result, we recommend that a 20% - 30% contingency be used in order to cover additional costs.

<b>ITEM COST SUBTOTAL:</b>		<b>\$44,388,000</b>
<b>Contingency:</b>	<b>30%</b>	<b>\$13,317,000.00</b>
<b>ITEM COST WITH CONTINGENCY:</b>		<b>\$57,705,000</b>
Mobilization:	10.00%	\$5,771,000
Utilities	5.00%	\$2,886,000
Right of Way	2.00%	\$1,155,000
Force Account Provision:	10.00%	\$5,771,000
<b>CONSTRUCTION SUBTOTAL:</b>		<b>\$15,583,000</b>
<b>Engineering and Environmental Fees</b>		
Design Fee:	10.00%	\$5,771,000
Environmental Clearance Fee:	2.00%	\$1,155,000
Construction Engineering	10.00%	\$5,771,000
<b>FEE SUBTOTAL:</b>		<b>\$12,697,000</b>
		<b>d = a+b+c</b>
<b>TOTAL PROGRAM COST:</b>		<b>\$86,000,000</b>

a

b

c

d

Costs highlighted in black are percentages applied to the Item Cost With Contingency (a)

Prepared by



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719-520-5800

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# Briargate-Stapleton Corridor Study

## Appendix F: Public Involvement Summary

El Paso County Department of Public Works

On-Call Contract: #17-067-51  
12/09/2021

**DRAFT**

# Briargate-Stapleton Corridor Study

## Appendix F: Public Involvement Summary



Prepared for

El Paso County Department of Public Works  
On-Call Contract: #17-067-51

**DRAFT**

December 9, 2021

Prepared by

**WILSON**  
& COMPANY

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719-520-5800

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# Attachment A – Project Website

## Project Website

A full-function website was developed for the project (go to: [Corridor Study | Briargate-Stapleton Project for Mobility](#)). The scrolling Home Page (see **Figure 1**) begins with a Welcome and Project News banner that includes links to frequently visited site Features. The website includes: a Project Overview, a library of Project Resources and a Questions & Answers posting (see **Figure 2**). Public and stakeholder input is facilitated by both an interactive Comment Map (see **Figure 3**) and an online Comment Form (see **Figure 4**).



Figure 1 Project Website - Front Page Banner

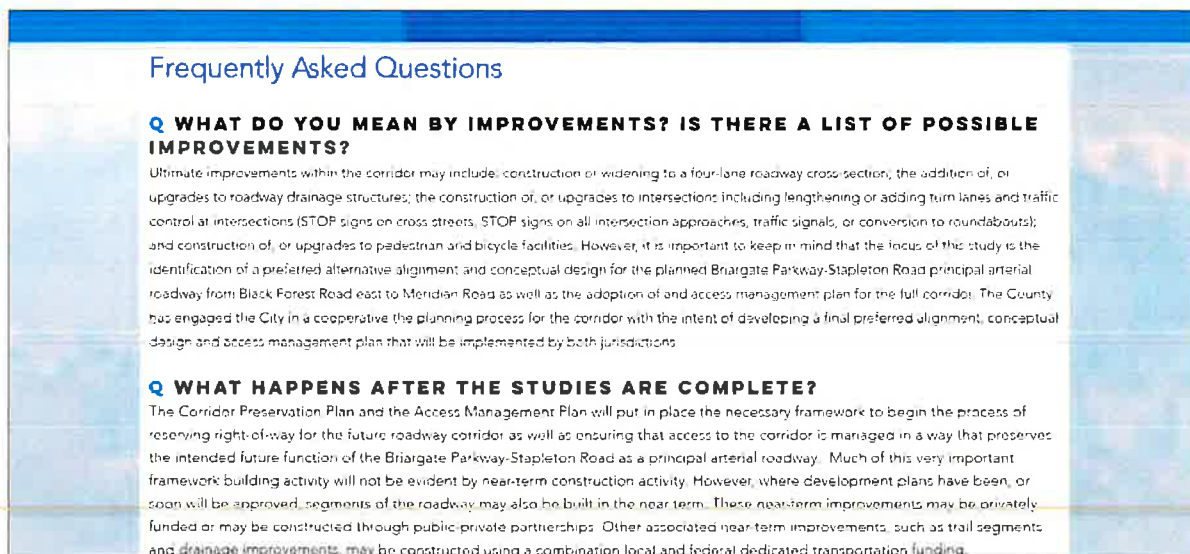


Figure 2 Website Frequently Q&A Posting

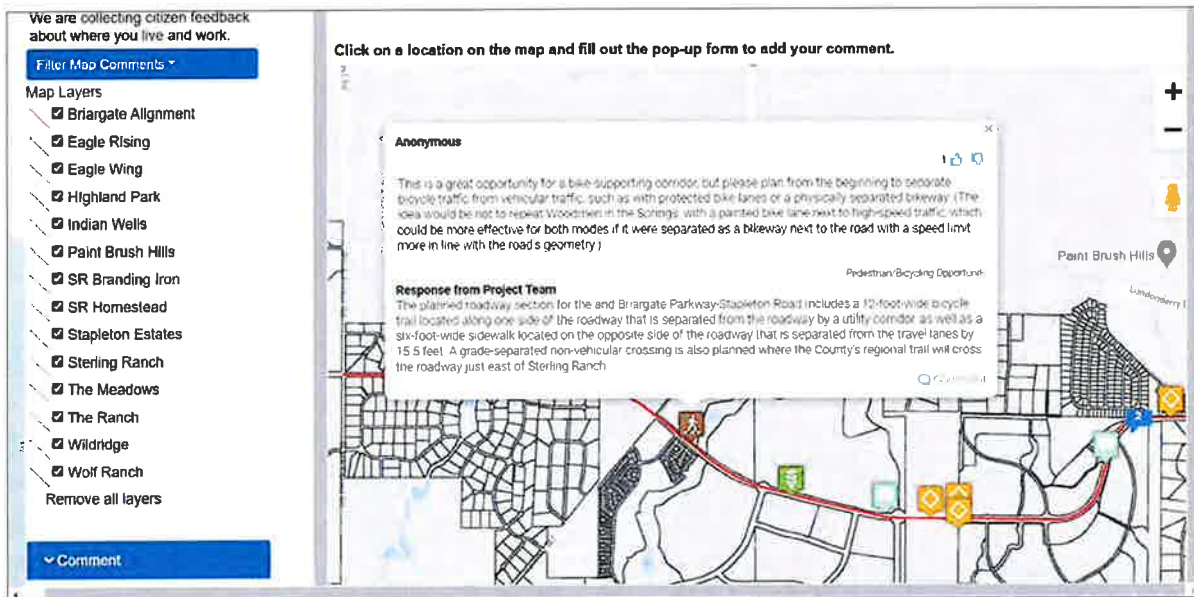


Figure 3 Website Comment Map – Example Comment and Response

## CONTACT

Maureen Paz de Araujo, FAICP  
Senior Transportation Planner/Project Manager  
Wilson & Company

Enter Your Name

Enter Your Email

Enter Your Subject

Message

☐ Add me to the project mailing list

Submit

Figure 4 Website Comment Form

## Attachment B – Virtual Open House

## The El Paso County Virtual Public Open House for the Briargate-Stapleton Corridor Study is Live!

The Virtual Open House is your opportunity to learn about the study, review the study's draft recommendations, submit your comments or questions to the project team, and provide valued input to help shape the final recommendations for the study. Visit the Open House at: [Virtual Public Open House](#)

Here are some helpful tips for the first-time virtual meeting attendees.

1. Use your mouse, or finger if you are using a device with touchscreen display, to pan around the room to the different exhibits.
2. Click on the informational button (blue square with the letter “i”) to learn about what information will be presented at that particular exhibit.
3. Review the information being presented at each exhibit by clicking on the other blue button at each exhibit.



The Virtual Public Open House will be virtual due to COVID-19 restrictions. Although the formal comment period for the Virtual Public Open House has closed, the site remains open for viewing. Before or after touring the Open House you can still use the [Comment Form](#) below to let us know your thoughts about the project. Alternatively, you can comment on specific locations along the corridor using the interactive comment map available on this website at: [Interactive Map](#). For full detail, you can check the boxes to the right of the interactive map to turn on embedded layout layers for the approved and planned developments along the corridor.



## Station #1 Info:

Welcome to the Briargate Parkway-Stapleton Road Corridor Planning Study Virtual Public Open House.

Take a virtual walk through the Open House exhibits to learn more about corridor preservation, access management and future improvements to the Briargate Parkway-Stapleton Road corridor, between Black Forest Road and Meridian Road. As part of the regional transportation network, this planned principal arterial roadway will provide improved connectivity within the greater Colorado Springs area and beyond.

- The Virtual Open House is your opportunity to:
- Learn about the corridor study,
- Review the study's draft recommendations,
- Submit comments or questions to the project team, and
- Provide valued input to help shape the final recommendations for the study.

Helpful tips for the first time virtual meeting attendees.

1. Use your mouse, or finger if you are using a device with touchscreen display, to pan around the room to the different exhibits.
2. Click on the informational button (blue square with the letter I) to learn about what information will be presented at that particular exhibit.
3. Review the information being presented at each exhibit by clicking on the other blue button at each exhibit.
4. Submit a comment form with your thoughts and a study team member may reach out to you to initiate a conversation.

Thank You for visiting our virtual open house and we look forward to hearing from you.

## Station #2 Info:

The Briargate Parkway–Stapleton Road (in some locations referred to as Stapleton Drive) corridor, between Black Forest Road and Meridian Road, is an integral part of a larger transportation corridor system providing connectivity with I 25 to the north and south of the greater Colorado Springs area. Stapleton Road continues easterly from this project area to connect with US 24 and with Judge Orr Road and Curtis Road to the east of the City. Curtis Road provides connection to Bradley Road and ultimately to I-25 south of Colorado Springs.

**Project Purpose:** The purpose for constructing an arterial roadway in the Briargate-Stapleton is to provide a continuous roadway connection between Interstate 25 and U.S. Highway 24 in northern El Paso County both for regional system connectivity and to serve the substantial transportation demand that is anticipated from planned development in this area.

**Project Need:** This portion of northern El Paso County is already experiencing substantial growth, and east-west roadway options in the area are extremely limited. Connections to Interstate 25 are limited for the six miles where it exists on United State Air Force Academy (USAF) property, between Academy Boulevard (Exit 150) and North Gate Boulevard (Exit 156). USAFA is a designated National Historic Landmark where no additional Interstate access will be granted. Briargate Parkway has access (Exit 151) and this roadway has sufficient capacity to accommodate the demand from planned urban development.

### **Station #3 Info:**

To determine the recommended horizontal alignment, research was conducted on plats that had been approved and development plans that had been submitted to either El Paso County or the City of Colorado Springs. Based on this research, two alternative alignments were developed and screened. Both alternatives begin on the west at Black Forest Road and follow the same alignment to Vollmer Road. At Vollmer Road, the northern alternative connects existing roadway segments and follows a direct route between Vollmer Road and Meridian Road. The southern alternative follows the northern alignment for continues to an alignment approximately half a mile south of the existing Stapleton Road before returning to the north and tying in with the existing road. The southern alignment more closely matched the corridors proposed on the submitted plats and was selected as the preferred alignment due to ROW constraints and its conformance with the submitted plats.

## **Station #4 Info:**

### **Roadway**

The El Paso County Urban Principal Arterial includes 2-12' thru lanes in each direction, with a 4' inside shoulder, 6' detached sidewalk, a 16' raised median, and outside curb and gutter for drainage. The City of Colorado Springs Principal Arterial includes a 17' raised median, 2-11' thru lanes in each direction, with a 6' outside shoulder, 6' detached sidewalk, and outside curb and gutter for drainage. These two standard sections were used to develop a hybrid proposed roadway section for the corridor.

### **Multimodal Facilities**

The following pedestrian and bicycle facilities are included in the proposed roadway section:

- Large multipurpose shoulders
- Separate bike path/multipurpose trail
- Detached sidewalks

A future grade-separated trail crossing (where County regional trail crosses just east of Sterling Ranch) is also under consideration.

### **Utilities**

The proposed roadway section includes a utility corridor. Colorado Spring Utilities has prioritized the corridor for a future gas line extension.

View Typical Section Exhibit to see proposed roadway design elements and planned phasing.

## Station #5 Info:

Access management alternatives, including access limitations, consolidation, and closures, were considered as means to preserve the functionality of the future roadway as a Principal Arterial. Proposed access locations were identified from master plan and overall development plan submittals on file. Because planning for a large parcel(s) west of Vollmer Road was not defined, a future direct access was located toward the west boundary of the property, providing maximum spacing between the Vollmer Road and Lochwinnoch Lane accesses.

The following access spacing criteria were used as the basis for access management planning

### 4-Lane Principal Arterial Access Criteria

#### El Paso County

Spacing of 1/2 mile between signalized intersections (2640')

#### City of Colorado Springs

- Spacing of 1/2 mile between signalized intersections (2640')
- Spacing of 1/4 mile between unsignalized intersections (1320')
- Same criteria for 6 lanes

View Map Exhibit to see proposed access locations for the Briargate Parkway - Stapleton Road study corridor.

### **Station #6 Info:**

At the Corridor Preservation Plan milestone of overall project development, quantified project impacts cannot be determined, but it is possible to identify the types of resources that would likely be affected, and to identify the general types of mitigation and permitting requirements that may apply. At this level, the following resources have been addressed:

- Floodplain Permitting
- Wetlands Mitigation and Permitting
- Water Quality Permits
- Farmland Protection
- Wildlife (Senate Bill 40 Certification)
- Hazardous Waste and Materials (Environmental Site Assessment)
- Noise Analysis
- Air Quality
- Wildflowers and Noxious Weeds

View Exhibit Mapping of resources and topics that will require additional analysis.

### **Station #7 Info:**

The Briargate/Stapleton corridor traverses three major drainage basins - Cottonwood Creek, Sand Creek, and Falcon Watershed. Data from available Drainage Basin Planning Studies (DBPS), Major Development Drainage Plans, and Final Drainage Reports was used to estimate the off-site drainage needs. Findings and recommendations include:

- Non-roadway (off-site) drainage traverses the Briargate/Stapleton corridor at approximately 30 locations.
- Roadway (on-site) drainage will require approximately 17 outfall (run-off collection) locations along the corridor.
- The most significant corridor crossing locations will be at Cottonwood Creek, Sand Creek, West Tributary of Falcon Watershed, and East Tributary of Falcon Watershed.
- Runoff from the roadway will need to be treated for water quality.
- Detention of runoff will be required to reduce flows to required levels.

View the Drainage Map Exhibit to see the locations of existing and planned drainage features.



## Station #8 Info:

### 2045 Traffic Forecasts

Forecast 2045 daily traffic flows for the project corridor range from 15,000 ADT to 20,000 ADT to the east of Lower Avenue and Black Forest Road, respectively. West of and west, respectively. These volumes are the 4-Lane Principal Arterial functional classification for the corridor that is specified by both the PPACG 2045 Moving Forward Regional Transportation Plan and the El Paso County 2040 MTCF. The PPACG and City of Colorado Springs plans specify a Principal Arterial with a six-lane cross section west of Black Forest Road, where forecast volumes pick up significantly to 35,000 ADT to 40,000 ADT to the west of Union Boulevard.

### Intersection Design Alternatives

Alternative intersection designs that will be evaluated for selected locations include the following:

- Right-In/Right-Out Only (RIRO) T-Intersection
- Full-Access Signalized Intersection
- Modern Roundabout Intersection

View the Traffic Map Exhibits to see forecast 2045 traffic, alternative intersection layouts, and the intersection layout alternatives that will be evaluated at each intersection.

### Station #9 Info:

Until funding for design and construction of Briargate Parkway-Stapleton Road (between Black Forest Road and Towner Avenue) is secured, the County will undertake the following steps to pave the way for future phased construction of the roadway:

- **Master Plan Amendment** - El Paso County will adopt a Master Plan Amendment to include adoption of the identified Briargate Parkway-Stapleton Road alignment, right-of-way footprint, and Access Management Plan
- **County-City Intergovernmental Agreement (IGA)** - The City of Colorado Springs (City) and El Paso County (County) will enter in to an IGA for the purpose of committing to jointly implement the study recommendations.
- **Right-of Way Preservation** – The City and County will acquire right-of-way for Briargate Parkway – Stapleton Road through the development review and approval process.
- **Funding** – The City and County will identify funding to construct Briargate Parkway Stapleton Road, between Towner Avenue and the east terminus of existing Briargate Parkway.

## **Station #10 Info:**

### **Tell Us What You Think - El Paso County Needs Your Input!**

Submit geographically specific comments using our Interactive Comment Map at: [Interactive Map](#)

Alternatively, you can sign up for our Mailing List and share your thoughts using our Comment Form at: [Project Web Site](#)

### **Frequently Asked Questions – We’ve Got the Answers!**

# Attachment C – Public Comments Summary

## Website Comment Form - Comment Tracking

Brangate Stapleton Stakeholder Input Tracking

Stakeholder email address

Meeting No.	Received Date	Received Time	Response Date	City/Town/Community	Location Description	Stakeholder 1	Stakeholder 2	Stakeholder 3	Stakeholder 5	Stakeholder Proposed Solution	Map / Community Remarks	Interview	Web/Video Contact	Phone Email	First name	Last name	Email	V/N Add to Project Meeting List?	Phone	Address	Notes
1	07/22/21	Monday 11:20 AM	08/04/21	Brangate	Stapleton	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway
2	07/19/21	Monday 8:30 AM	08/04/21	Brangate	Stapleton	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway
3	07/16/21	Tuesday 1:00 PM	08/04/21	Brangate	Stapleton	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway
4	07/06/21	Monday 12:15 PM	08/04/21	Brangate	Stapleton	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway
5	06/26/21	Tuesday 2:30 PM	08/04/21	Brangate	Stapleton	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway
6	06/09/21	Tuesday 1:00 PM	08/04/21	Brangate	Stapleton	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway
7	06/01/21	Tuesday 10:00 AM	08/04/21	Brangate	Stapleton	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway
8	05/07/21	Wednesday 10:00 AM	08/04/21	Brangate	Stapleton	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway
9	05/07/21	Monday 11:00 AM	08/04/21	Brangate	Stapleton	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway	Brangate Parkway







## Website Comment Map - Comment Tracking

Brigade-Stapleton Stakeholder Input Tracking  
Community Remarks: No stakeholder emails

Record No.	Received Date	Received Time/Day	Response Date	City/Town/Community	Location Description	Roadway 1	Roadway 2	Roadway 3	Roadway 4	Issue	Stakeholder Proposed Solution	Method of Contact				First name	Last name	Email	VIN Add as project Meeting Link?	Phone	Address	Notes
												Community Meeting / Remarks	Website Contact	Form Email	Cell Phone							
1	04/09/21	Wednesday 2:53:00 PM								Why is this connection necessary? We did not want to build a new road, but we need a major transportation corridor that is what Woodmen Road is for. This portion of Stapleton borders multiple single family residences. People thought to live in this area and build a new road. It's a good idea, but we need to make sure it's a good idea for everyone. We need to make sure it's a good idea for everyone.						Mitchell	Hess					See Record 6 in Stakeholder Emails Excel sheet and date: email response in Word doc.
2	04/09/21	Wednesday 7:57 PM			Learning Ranch and The Ranch developments					Consider the Learning Ranch and The Ranch developments. These two parcels. What is the plan to connect the road over this area? Will there be an impact on the existing traffic? Will there be an impact on the existing traffic?						Anonymous	Anonymous					
3	04/14/03	Tuesday 5:06 AM			Entire project	Woodmen Road				This proposed route traverses the same east-west region as Woodmen Road already does. It would provide a section of Woodmen Road for a more direct route to the area. It would provide a more direct route to the area. It would provide a more direct route to the area.	Improve Woodmen Road only					Steve	Schuck					
4	04/11/21	Tuesday 8:02 AM			Entire project	Burgess Road	Volmer Road			Oppose extension of Burgess-Stapleton because "Existing houses, planned neighborhoods, and commercial trails & parks are already being brought to the area." "As a local resident who uses Stapleton Road daily, I don't want it becoming the Woodmen Road in a later stage of the project. I don't want it becoming the Woodmen Road in a later stage of the project."	"It would be better to use Burgess or Volmer that are not already congested with pedestrians and children."					Anonymous	Anonymous					See Stakeholder Emails Excel sheet 1 record number 16. Comment here made by Anonymous, but Stakeholder is likely Sierra Hess as comment is really identical and made on same date only 2 minutes later.
5	04/11/21	Thursday 7:55 AM			Entire project					This is a great opportunity for a bike-supporting corridor, but please plan from the beginning to ensure that it is a safe route for all modes of transportation. It should be a safe route for all modes of transportation. It should be a safe route for all modes of transportation.						Anonymous	Anonymous					
6	04/15/21	Thursday 9:44 PM								Ensure beyond safety						Anonymous	Anonymous					





## Attachment D Stakeholder Coordination

## Stakeholder Coordination 03/25/2020 Alignment





## Stakeholder Collaboration - Typical Section

**To:** Howard Schwartz, El Paso County

**From:** Justine Fedde, Wilson & Company, Inc., Engineers & Architects

**CC:** Maureen Paz de Araujo

**Date:** 4/14/2020

**Re:** Stapleton Briargate Cross Section and Access Control

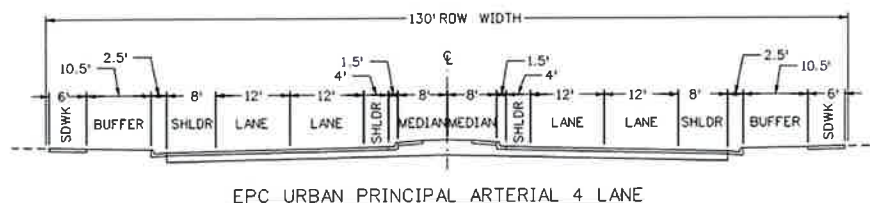
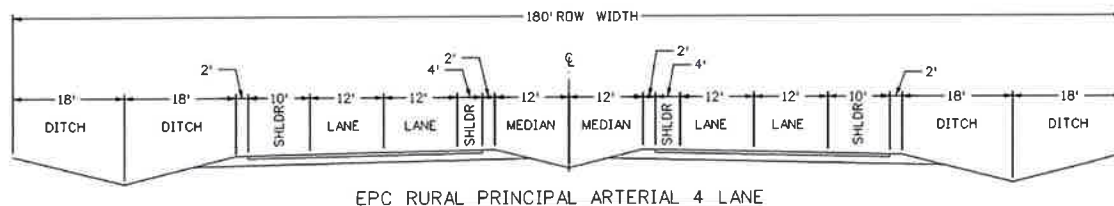
The Briargate Parkway/Stapleton Road Corridor Project is a mostly undeveloped corridor with some portions containing existing roadways of various types and phases associated with adjacent development in El Paso County (EPC). The project area begins at Black Forest Road (eastern boundary of both Wolf Ranch and the City of Colorado Springs). The project ends at Meridian Road on the west. The project area is anticipated to have significant development with an expectation that some or all of the project will be annexed into the City of Colorado Springs (COS) in the future. An overall project vicinity map is provided in **Exhibit 1**.

## Traffic Volumes

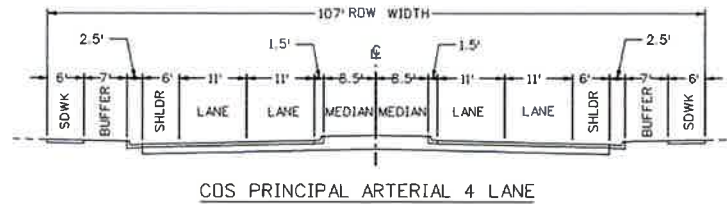
The unadjusted 2045 forecast volumes (20,000 ADT) are compatible with a 4-lane Principal Arterial in the corridor. This is consistent with the classification in the PPACG regional model. The model is coded with 4 lanes east of Black Forest Road and 6 lanes west of Black Forest Road, where volumes pick up significantly – more in 30,000 ADT range.

## Roadway Typical Section

The EPC Major Transportation Corridors Plan shows a future Principal Arterial with four lanes between Judge Orr Road and Black Forest Road. This typical section includes 2-12' thru lanes in each direction, with a 4' inside shoulder. The rural section included a 10' outside shoulder, a depressed 24' median and graded ditches for drainage. The urban section includes a 4' inside shoulder, 6' detached sidewalk, a 16' raised median, and outside curb and gutter for drainage.



West of Black Forest Road, the City's plan shows a Principal Arterial. This typical section includes a 17' raised median, 2-11' thru lanes in each direction, 6' outside shoulder, 6' detached sidewalk, and outside curb and gutter for drainage.

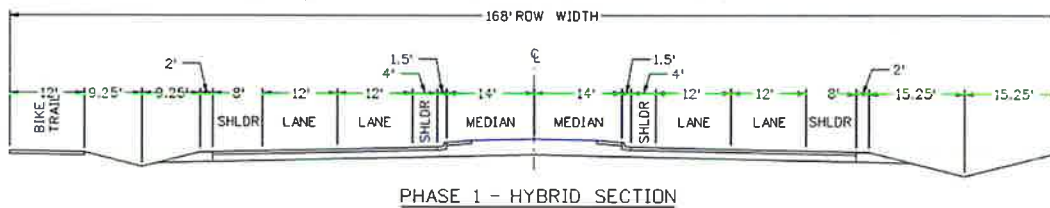


## Proposed Hybrid Section

Due to the forecasted traffic volumes in this area, it is recommended to use a hybrid of EPC's urban and rural Principal Arterial sections and the COS Principal Arterial section. Below are the recommended 4-lane cross sections for the phased construction of this project. Six lane sections would be similar but would add an extra 12' lane in both phases to the outside.

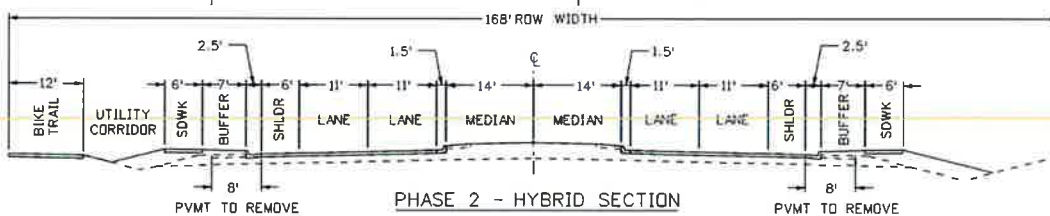
### Phase 1

As development occurs, the Stapleton/Briargate road can grow to meet development demands. Phase 1 will more closely resemble an EPC typical section with a 28' raised median, to allow for double left turn lanes, inside curb and gutter, 4' inside shoulder, 2-12' thru lanes in each direction, an 8' outside shoulder and graded ditches for drainage. Additionally, 12' bike trail would be included on the edge of the right of way. This bike path would be separated from the sidewalk by a dedicated utility corridor. Phase 1 could be split into two phases for initial construction. In Phase 1A, one direction of travel would be constructed. Then the roadway could be opened for two-way, single lane travel while Phase 1B was constructed.



### Phase 2

The second Phase of Construction builds within the ROW initially set aside and upon what was constructed in Phase 1. Phase 2 will more closely resemble the COS typical section with 11' thru lanes in each direction, and a 6' outside shoulder. In Phase 2, the outside edge would be defined by a curb. The 6' outside shoulder would allow for shared facility bicycle path while the 6' separated sidewalk ensures increased pedestrian safety. Phase 2 would require the removal of 8' of Phase 1 pavement from each side of the section.



## Access Control

Both the EPC Engineering Criteria Manual and the COS Traffic Criteria Manual permit intersections along a Principal Arterial to be spaced at half-mile intervals. EPC does not permit access to Principal Arterials between intersections. COS allows for one access drive per property ownership which may be jointly shared with adjacent properties. COS permits median cuts at a spacing between a quarter-mile and a half-mile, at major or significant street intersections.

### **2.2.2 Circulation**

Circulation is important in transportation system design for the following reasons:

- Operation of the arterial road system is improved by dispersing local traffic onto multiple roads and access points;
- Local roads are to be used only for accessing developed lots;
- Response time for emergency services is reduced;
- Time and mileage traveled by individuals and service providers, including school bus transportation, mail delivery, utilities, etc., are reduced; and
- Use of transit systems, and pedestrian and bicycle facilities, is promoted.

### **2.2.3 Transportation Impact Study (TIS)**

#### **A. General**

The goal of the TIS is to identify the traffic-related issues that result from development and to determine mitigation techniques required to maintain acceptable levels of service, meet the transportation planning goals, and implement the El Paso County Major Transportation Corridors Plan (MTCP).

#### **B. TIS Preparation Guidelines**

A TIS shall be prepared in accordance with the general guidelines in Appendix B. A number of specific parameters shall be evaluated in the TIS based on the level of complexity of the development and location within the transportation network. Three levels of analysis are permitted. Prior to assuming a specific level of required analysis, the design engineer should consult with the ECM Administrator to determine what level of analysis is appropriate and desired in support of a specific project proposal.

#### **C. Qualifications to Prepare a TIS**

A TIS shall be prepared under the supervision of, and sealed by, a licensed Professional Engineer in the State of Colorado with experience in traffic engineering and transportation planning.

### **2.2.4 Roadway Functional Classifications and Urban/Rural Designations**

Roadway functional classification is one parameter used to determine appropriate road design. The function of a road is determined by the volume of traffic, length of vehicle trips, and whether the road provides service primarily for vehicular movement or access to abutting land uses. For example, arterial roadways generally carry significantly greater traffic volumes and variety of traffic types at higher speeds than collector roads. Similarly, collector roads will carry greater traffic volumes at higher speeds than local roads.

Roadway functional classifications for regional based facilities are established by the most recently adopted MTCP. Other roadways are classified by the BOCC based on whether the adjoining land uses are rural or urban in nature (i.e. developments with lots greater than or equal to 2.5 acres), along with the existing and projected objectives of the roadway.

The County recognizes six roadway functional classifications within the rural designation: expressways, principal arterials, minor arterials, major collectors, minor collectors, and locals. The County recognizes seven roadway functional classifications within the urban roadway designation: expressways, principal arterials, minor arterials, nonresidential collectors, residential major collectors, residential minor collectors, and locals.

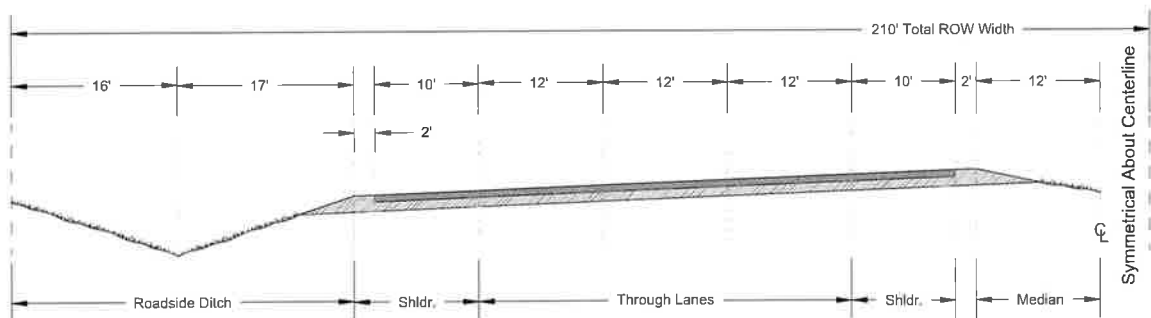
These Standards have been developed in support of the County roadway functional classification system.

#### A. Rural Roadways

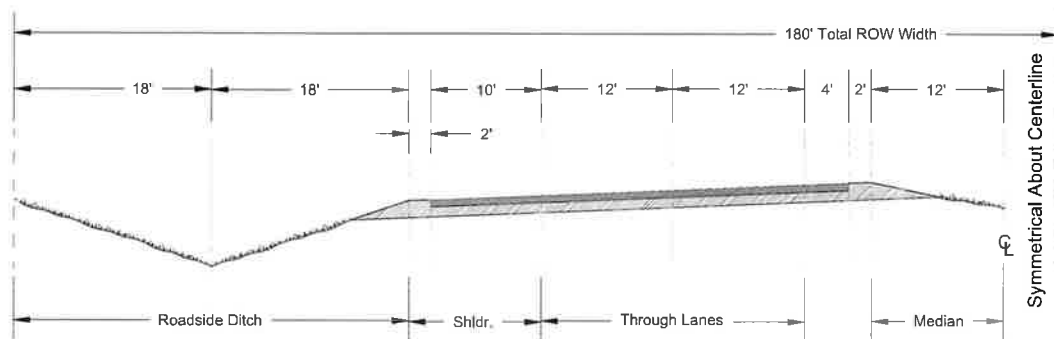
##### 1. Expressway

Expressways serve high-speed and high-volume traffic over long distances. Access is highly controlled and may have both grade-separated interchanges and full movement signalized intersections. Adjacent, existing and future, land uses shall be served by other network roadways, no direct parcel access is permitted (See Figure 2-1 and Figure 2-2).

**Figure 2-1. Typical Rural Expressway Partial Cross Section (6 Lane)**



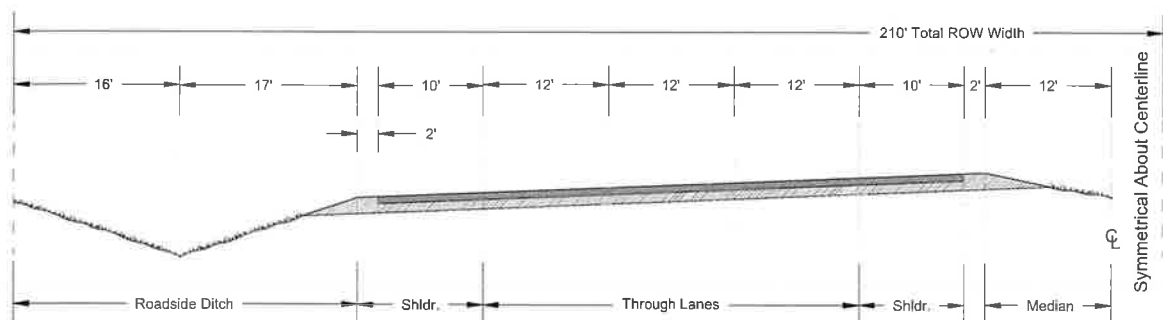
**Figure 2-2. Typical Rural Expressway Partial Cross Section (4 Lane)**



## 2. Principal Arterial

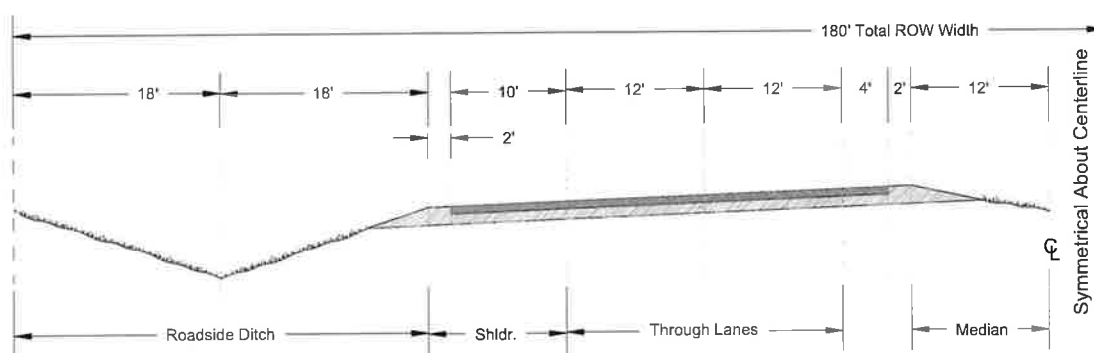
Principal arterials serve high-speed and high-volume traffic over long distances. Access is highly controlled with a limited number of full movement intersections and medians with infrequent openings, and no direct parcel access. Adjacent, existing and future, land uses shall be served by other network roadways, service roads, and inter parcel connections (See Figure 2-3 and Figure 2-4).

**Figure 2-3 Typical Rural Principal Arterial Partial Cross Section (6 Lane)**





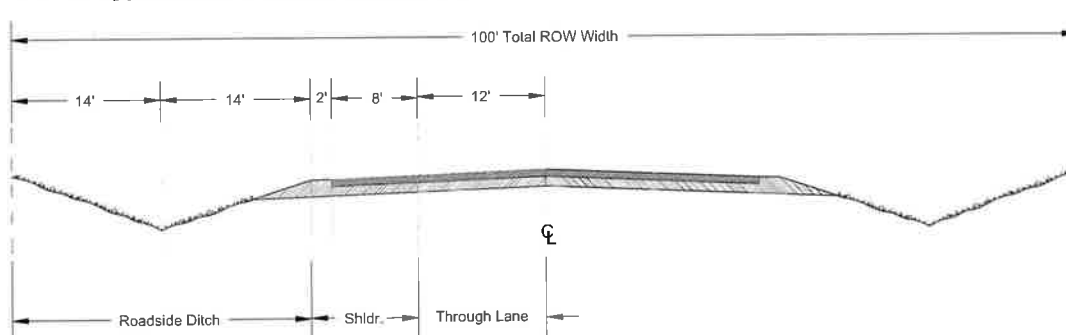
**Figure 2-4. Typical Rural Principal Arterial Partial Cross-Section (4 Lane)**



### 3. Minor Arterial

Minor arterials serve high-speed and high-volume traffic over medium distances, or are anticipated to serve this kind of traffic within a twenty-year period. Access is restricted through prescribed distances between intersections, use of medians, and no full movement parcel access (See Figure 2-5). Minor arterial status is assigned to rural roadways where the probability of significant travel demand in the future is high. Rights-of-way, easements, setbacks, and access limitations shall be pursued through the land development process on properties adjacent to minor arterials.

**Figure 2-5. Typical Rural Minor Arterial Partial Cross Section**

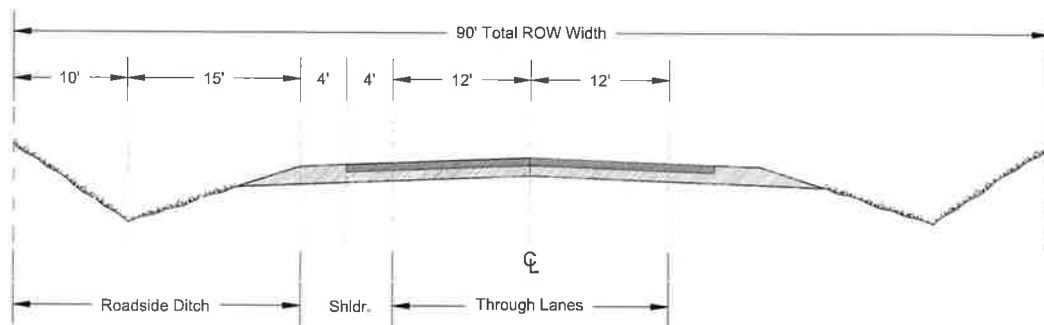


### 4. Major Collector

Major collectors serve as links between local access and arterial facilities over medium-to-long distances. Major collectors are managed to

maximize the safe operation of through-movements at speed. No full movement access is permitted where the local roadways can be expected to provide access (See Figure 2-6). Where no local public roadway can provide access, temporary direct parcel or partial turn movement access may be permitted, provided the design meets requirements presented in these Standards.

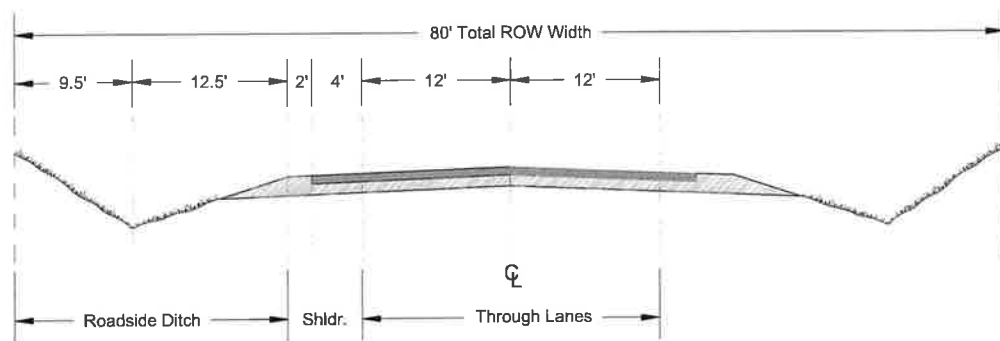
**Figure 2-6. Typical Rural Major Collector Cross Section**



## 5. Minor Collector

Minor collectors link local roadways to major collectors or arterial roadways. No full movement access is permitted where local roadways can be expected to provide access (See Figure 2-7). Where no local public or private roadway can provide access or where lot size is five acres or more, temporary direct parcel or partial turn movement access may be permitted. Access location and design are to be reviewed by the ECM Administrator to ensure roadway objectives are being met.

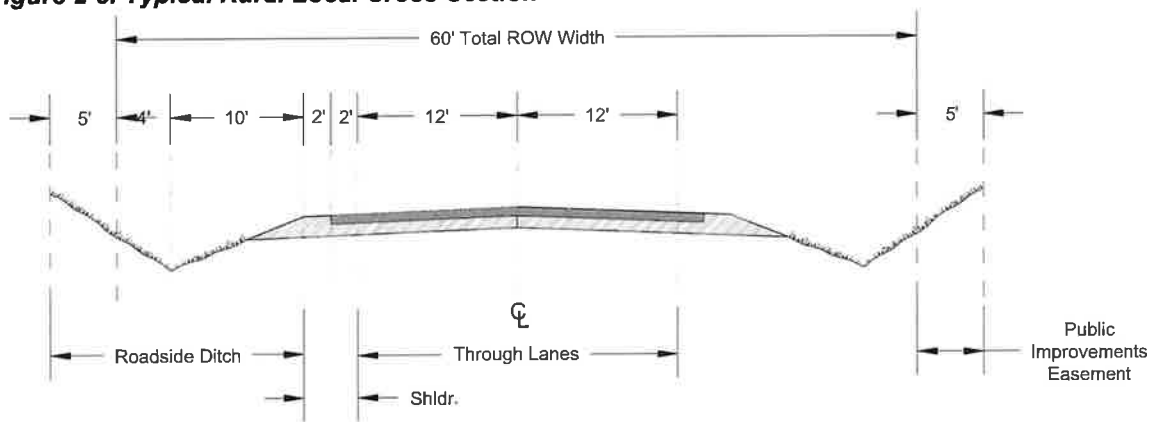
**Figure 2-7. Typical Rural Minor Collector Cross Section**



## 6. Local

Local roadways provide direct lot access and deliver lot-generated trips to collector roadways. Although access needs are high, accesses shall not be allowed to compromise the safety, health or welfare of roadway users (See Figure 2-8).

**Figure 2-8. Typical Rural Local Cross Section**

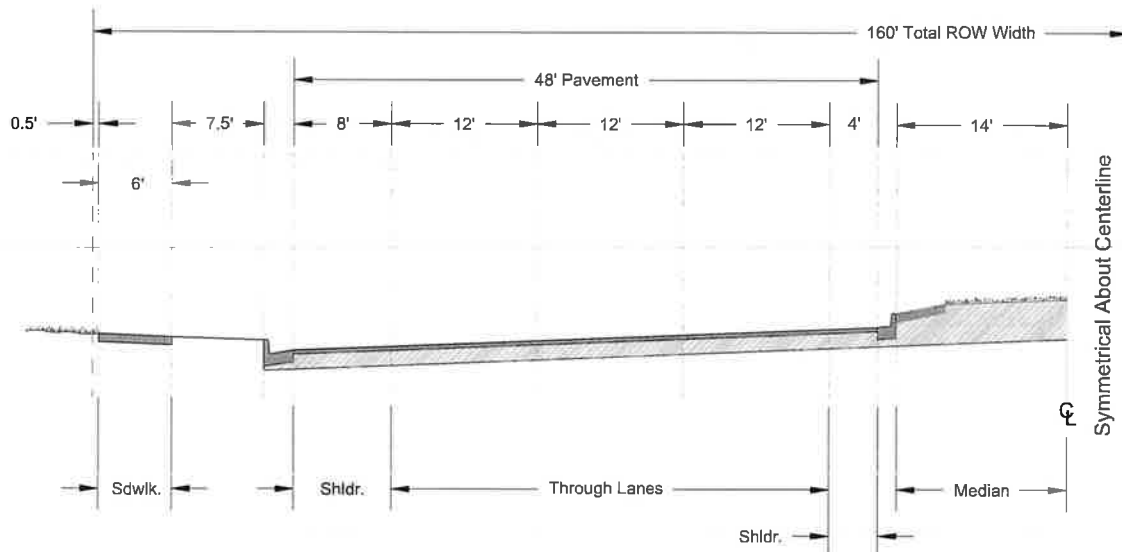


## B. Urban Roadways

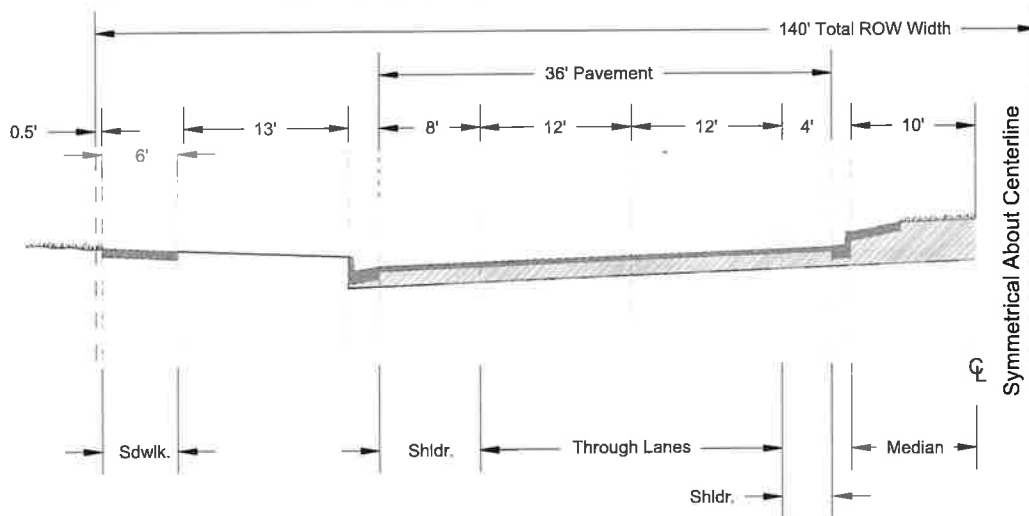
### 1. Expressway

Expressways serve high-speed and high-volume traffic over long distances. Access is highly controlled and may have both grade-separated interchanges and full movement signalized intersections. Adjacent, existing and future, land uses shall be served by other network roadways, no direct parcel access is permitted (See Figure 2-9 and Figure 2-10).

**Figure 2-9. Typical Urban Expressway Cross Section (6 Lane)**



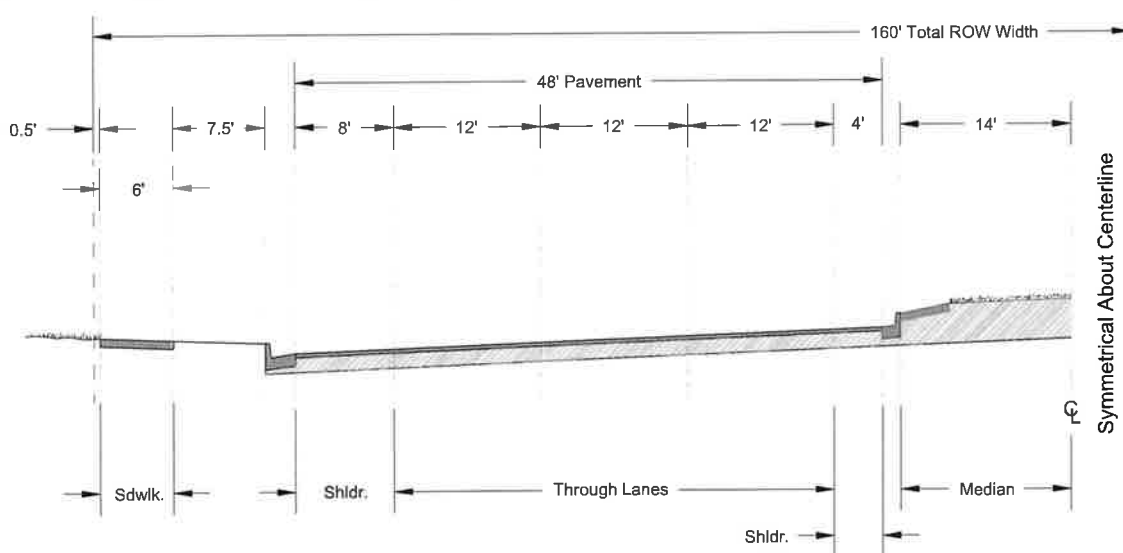
**Figure 2-10. Typical Urban Expressway Cross Section (4 Lane)**



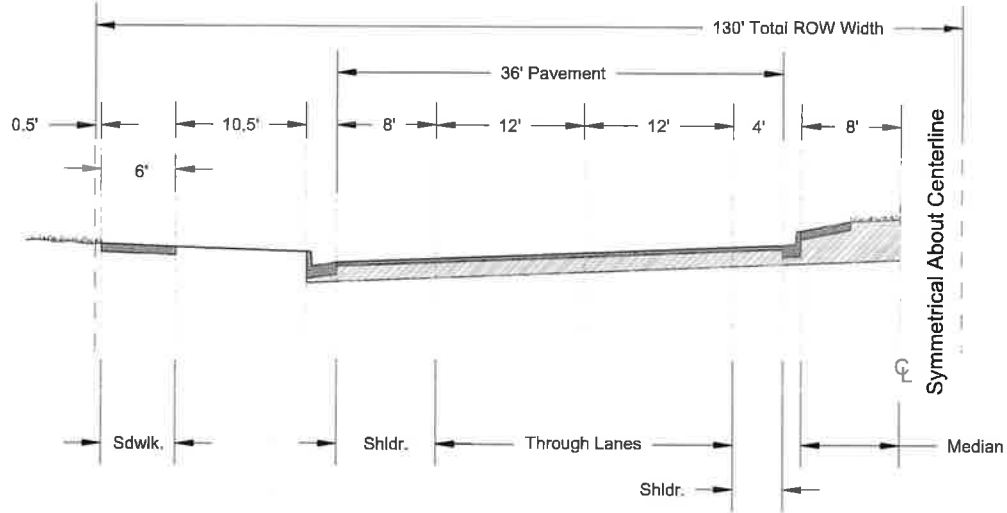
## 2. Principal Arterial

Principal arterials serve high-speed and high-volume traffic over long distances. Access is highly controlled with a limited number of full movement intersections and medians with infrequent openings, and no direct parcel access. Adjacent, existing and future, land uses shall be served by other network roadways, service roads, and inter parcel connections (See Figure 2-11 and 2-12).

**Figure 2-11. Typical Urban Principal Arterial Cross Section (6 Lane)**



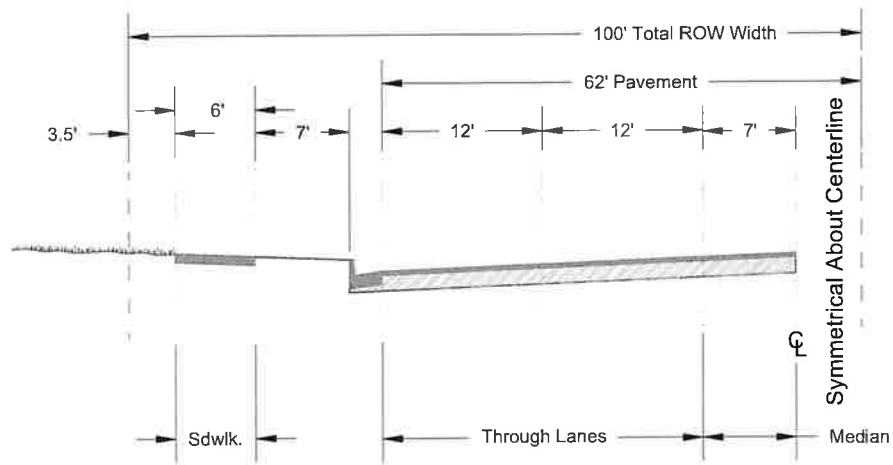
**Figure 2-12. Typical Urban Principal Arterial Cross Section (4 Lane)**



### 3. Minor Arterial

Minor arterials serve high-volume traffic over medium distances in developed or developing urban areas. Access is restricted and based on prescribed distances between intersections, use of medians, and no full movement parcel access is permitted where the local roadways can be expected to provide access (See Figure 2-13). Where no local public or private roadway can provide access, temporary lot or partial turn movement access may be permitted, provided the design meets these Standards.

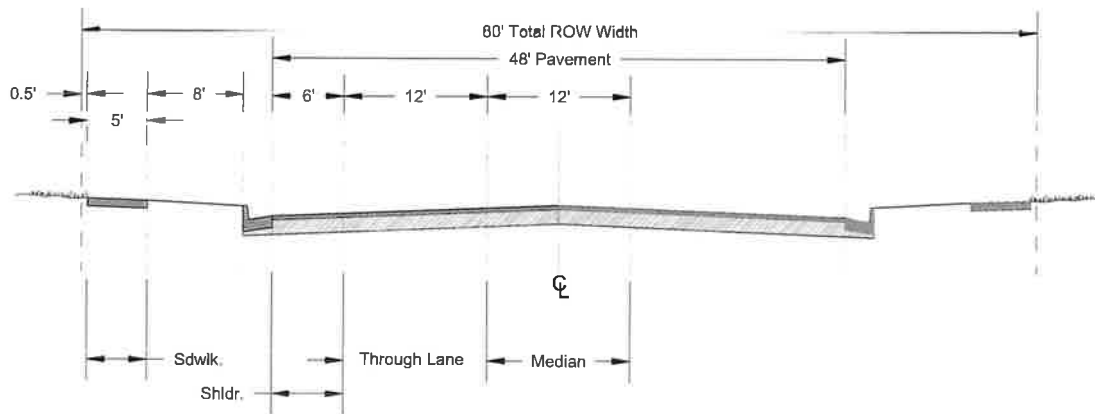
**Figure 2-13. Typical Urban Minor Arterial Cross Section**



#### **4. Nonresidential Collector**

Nonresidential collectors link local roadways and arterial roadways in locations characterized by nonresidential land uses or uncommitted future land uses. Urban nonresidential collectors are intended to accommodate multiple modes of transportation, high-volume turning movements or significant changes in roadway use over time (See Figure 2-14). Urban nonresidential collectors are managed to maximize the safe operation of through-movements. Intersection and parcel access locations and design are reviewed by the ECM Administrator to ensure roadway objectives are being met.

**Figure 2-14. Typical Urban Nonresidential Collector Cross Section**



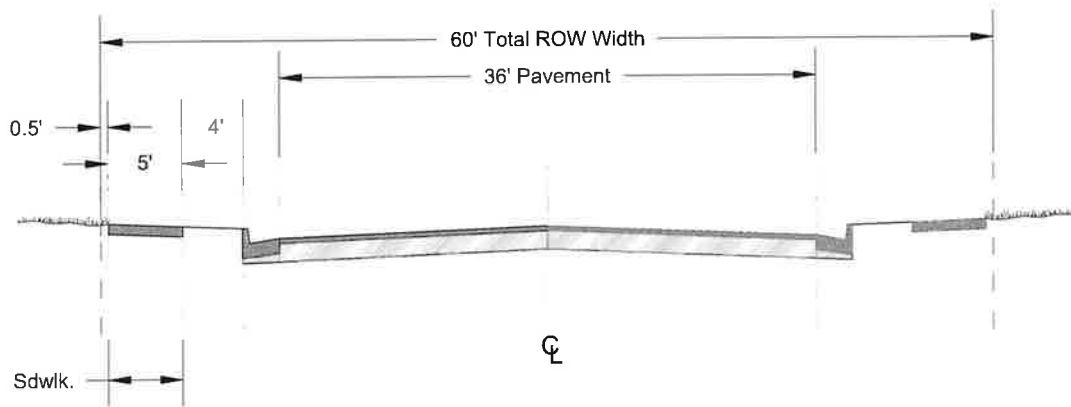
## 5. Residential Collector

Residential collectors link local and arterial roadways in exclusively residential areas where build-out conditions for land development and roadway use can be reasonably forecasted (See Figure 2-15).

Residential major collectors are managed to maximize the safe operation of through-movements. No full movement parcel access is permitted where the local roadways can be expected to provide access. Where no local public or private roadways can provide access, partial turn movement access may be permitted. Intersection and access location and design are reviewed by the ECM Administrator to ensure roadway objectives are being met.



**Figure 2-15. Typical Urban Residential Collector Cross Section**

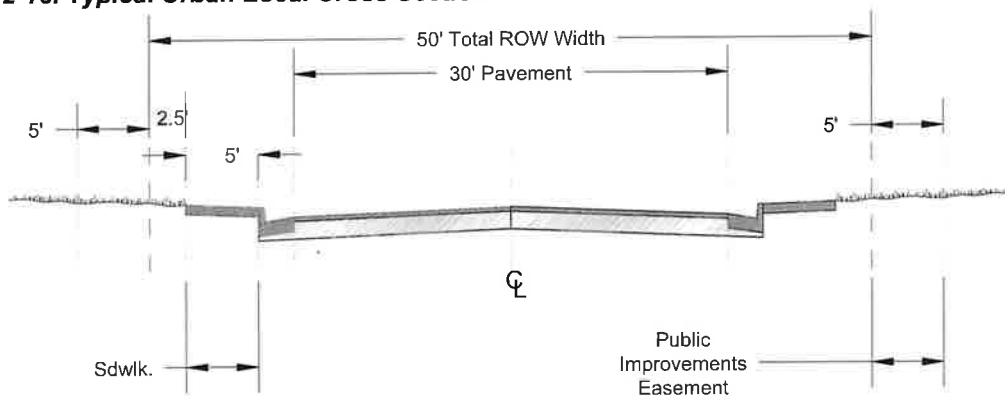


## 6. Local

Local roadways link to collector or arterial roadways. Direct parcel access is permitted provided they meet sight distance and other design requirements presented in these Standards (See Figure 2-16).

Intersection and parcel access location and design are reviewed by the ECM Administrator to ensure safe operations.

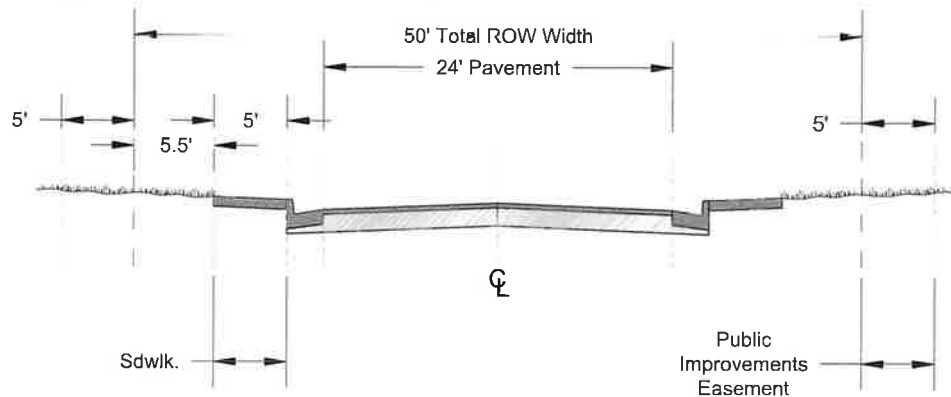
**Figure 2-16. Typical Urban Local Cross Section**



## 7. Local (low volume)

Local (low volume) roadways provide direct lot access and deliver lot-generated trips to collector roadways. Although access needs are high, accesses shall not be allowed to compromise the safety, health or welfare of roadway users (See Figure 2-17).

**Figure 2-17. Typical Urban Local (low volume) Cross Section**



### **2.2.5 Roadway Access Criteria**

All new or modified accesses to the County roadways shall meet the requirements of the ECM. Standards and technical criteria not specifically addressed in the ECM shall follow the provisions of the AASHTO, A Policy on Geometric Design of Highways and Roadways ("Green Book") and the Colorado State Highway Access Code. In addition, should any access request fall within the preview of the Major Thoroughfare Task Force (MTTF), per their adopted bylaws, then the request shall be brought before the MTTF for a recommendation.

#### **A. Rural and Urban Expressway Access Criteria**

##### **1. Intersection Spacing and General Access Standards**

Full movement intersections and major access spacing shall meet the requirements of this section. Right-in/right-out and three quarter movement accesses may be permitted as a deviation only if they meet the criteria presented in this section for sight distances, turn lane requirements, grades and do not negatively impact traffic operations or safety.

##### **2. No Alternative Access to Road System**

Where reasonable access can be obtained from the local roadway system, a temporary direct lot or partial turn movement access may be permitted provided the access meets these Standards or as otherwise required by the ECM Administrator.

##### **3. Access and Lot Division**

No additional access right shall accrue and no additional access shall be provided when splitting or dividing of existing lots of land. When an alternative is reasonably available in the opinion of the ECM Administrator, all access to the newly created properties shall be

# 15.0

## Roadway Standards

The City of Colorado Springs uses a roadway hierarchy to provide safe and continuous travel and access. A functional classification of roadways provides the hierarchy needed to accomplish this goal. Streets are divided into categories with different design criteria to maintain and protect the primary purpose of the roadway. The roadway standards provided in this manual are to provide a standard for street sections, but variances to these sections can be requested. Roadway reductions will be considered if a lesser cross-section can be shown to adequately accommodate the projected long-term traffic volumes. The cross-sections provided in this manual are illustrative only. There are not to scale and are not intended to be used as engineering design. For simplicity, some design elements such as sidewalks or bicycle lanes have been left off certain figures. The functional classifications are described below:

### 15.1

#### Standards for Freeways

1. **Function** - Freeways permit rapid and unimpeded movement of traffic through and around the City
2. **Right-of-Way Widths** - 420 feet, minimum with frontage roads, 332 feet, minimum without frontage roads
3. **Number of Moving Lanes** - Six to eight lanes
4. **Access Conditions**
  - a. Access shall be grade separated
  - b. Interchanges shall be made with major arterial streets and freeways only
  - c. No intersections at grade shall be permitted
5. **Traffic Characteristics**
  - a. No traffic signals
  - b. Parking prohibited
  - c. Bicycles prohibited
  - d. Two separate one-way roadways with a dividing median strip
6. **Planning Characteristics**
  - a. Freeways should connect with main highways approaching and leaving the City from all directions
  - b. Freeways should be so aligned as to serve the major traffic generators within the City, such as the central business district, major industrial areas, regional shopping centers, etc.
  - c. Freeways should not bisect neighborhoods or communities but should act as boundaries between them
  - d. Added right-of-way is provided for landscaping, grass planting, added safety, and noise attenuation

e. *At interchange areas:*

1. For diamond interchanges, right-of-way should flare to 580' in width from 1000' each side of the intersection right-of-way line at the intersecting cross street.
2. For cloverleaf interchanges, right-of-way should flare to 1300' in width from 1300' each side of the intersection right-of-way line at the intersecting cross street.
3. For alternative interchange designs (SPUI's, flyovers, etc.) ROW requirements will be provided based on the design.

7. **Design Characteristics**

a. *Grades*

1. Not less than one percent (1%) on tangents; nor more than four percent (4%)

b. *Cross sections*

1. Cross sections will be determined by specific design

## 15.2 Standards for Expressways

1. **Function** - Expressways permit rapid and relatively unimpeded movement of traffic through and around the City.
2. **Right-of-Way** - 210 feet minimum, with additional right-of-way for frontage roads, if required
3. **Number of Moving Lanes** - Four to six lanes
4. **Access Conditions**
  - a. *Access* shall be completely controlled
  - b. *Interchanges* shall be made with freeways and may be made with major arterial streets.
  - c. *Signalized* intersections are permitted with arterial streets only, preferably with one mile spacing. Signalized intersections at ½ mile locations may be allowed by City Engineering.
  - d. *Turn* restricted intersections may be allowed at half mile spacing or where they can be shown to benefit operations on the expressway.
  - e. High density or congested areas may require specific access plans.
5. **Traffic Characteristics**
  - a. *Traffic* control devices and channelization shall be provided at each intersection at grade.
  - b. *Parking* prohibited
  - c. *Two* separate one-way roadways with a dividing raised median
  - d. *Bicycles* may be permitted on the shoulder or on separated bikeways for portions of expressways.

## 6. Planning Characteristics

- a. *Expressways* should connect with main highways approaching and leaving the City from all directions.
- b. *Expressways* should be so aligned as to serve the major traffic generators within the City, such as the central business district, major employment centers, military installations, regional shopping centers, etc.
- c. *Expressways* should not bisect neighborhoods or communities but should act as boundaries between them
- d. *Added right-of-way* is provided for landscaping, grass planting, added safety, and noise attenuation
- e. *At interchange areas:*
  1. For diamond interchanges, right-of-way should flare to 580' in width from 1000' each side of the intersection right-of-way line at the intersecting cross streets
  2. For cloverleaf interchanges, right-of-way should flare to 1300' in width from 1300' each side of the intersection right-of-way line at the intersecting cross street
  3. See Freeway #3

## 7. Design Characteristics

- a. *Grades*
  1. Not less than one percent (1%) on tangents or more than four percent (4%)
- b. *Cross section*
  1. Cross sections will be determined by specific design

# 15.3

## Standards for Principal Arterial Streets

1. **Function** - Major arterial streets permit rapid and relatively unimpeded traffic movement throughout the City and carry high volumes of inter and intra traffic which connects major land use elements as well as communities with one another. Major function is to serve through traffic. The secondary function is to serve abutting property. This functional description pertains to four lane and greater facilities.
2. **Right-of-Way Width** - 107 - 142 feet
3. **Number of Moving Lanes** - Four to six lanes, with left and right turn bays. Right turn lanes shall be constructed at intersections of all arterial streets.
4. **Access Conditions**
  - a. *Intersections* and curb cuts shall be limited, as approved by City Engineering.
  - b. *Signalized intersections* shall be limited to 1/2 mile spacing unless adequate justification is provided to approve signalized intersections at other locations.
  - c. *Median cuts* will be permitted at major or significant street intersections, generally at intervals of approximately ¼ to ½ miles as approved by City Engineering.

## 5. Traffic Characteristics

- a. Regulation of traffic shall be accomplished by traffic control devices and channelization.
- b. On-street parking prohibited
- c. Vertical curb required with detached sidewalks
- d. Median shall be raised with curb and gutter (City Standard Detail D-6 and D-24)
- e. High density or congested areas may require specific access plans.

## 6. Planning Characteristics

- a. Principal arterial streets should be spaced approximately one mile apart in the suburban areas of the City to ¼ mile apart in areas of high population density and intense land usage.
- b. Principal arterial streets should not bisect neighborhoods, but should act as boundaries between them.
- c. Sidewalks shall be set back from the street.
- d. In general, abutting properties should not face on the roadway unless separated from it by a frontage road. Lots that directly abut the arterial shall have a minimum depth of 120 feet.
- e. Bicycle access shall be part of a 4' multi-use shoulder.

## 7. Design Characteristics

- a. Grades
  1. Not less than one percent (1%) on tangents; nor more than four percent (4%). Grades of six percent (6%) may be considered for unique, short distances.
- b. Alignment
  1. Horizontal – 1040 foot minimum radius at centerline for standard crowned cross-slopes
  2. Vertical – minimum length equivalent to K value times the algebraic difference in the rate of grade.

Figure 10

## Cross Sections for Principal Arterial Streets

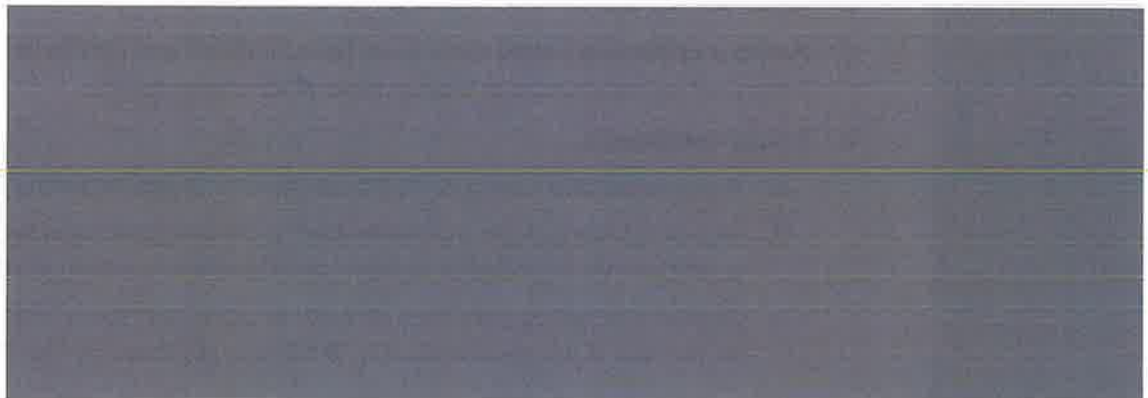
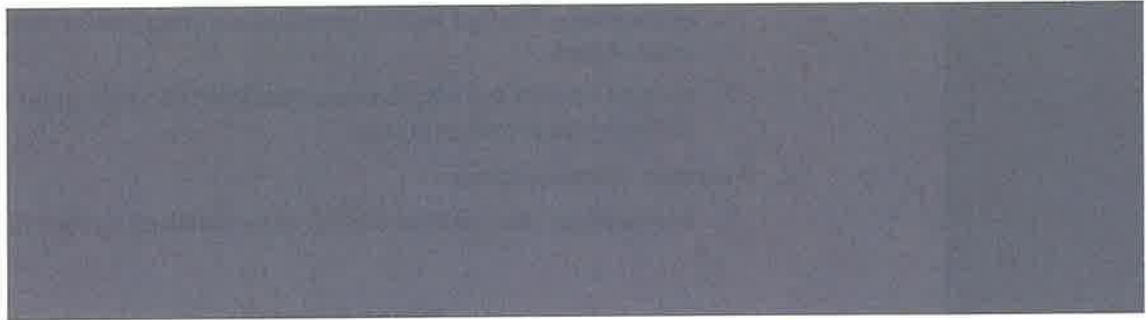


Figure 11

## Type II (6-Lane, 142' Right-of-Way)



## 15.4

### Standards for Minor Arterial Streets

1. **Function** - Minor arterial streets permit rapid and relatively unimpeded traffic movement throughout the City and carry high volumes of inter and intra-traffic which connect major land use elements.
2. **Right-of-Way Width** - 90 feet (with two 5 foot easements)
3. **Number of Moving Lanes** - Four lanes
4. **Access Conditions**
  - a. *Intersections* will generally be "T" type at grade as approved by City Engineering.
  - b. *Intersections* and curb cuts shall be limited as approved by City Engineering.
5. **Traffic Characteristics**
  - a. *Regulation* of traffic shall be accomplished by traffic control devices and channelization.
  - b. *On-street* parking prohibited
  - c. *Vertical* curbs required with detached sidewalks
  - d. *Medians* will be raised. Painted medians will be considered with adequate justifications.
6. **Planning Characteristics**
  - a. *Minor* arterial streets should be spaced approximately one mile apart in the suburban areas of the City to a few blocks apart in areas of high population density and intense land usage.
  - b. *Minor* arterial streets preferably should not bisect neighborhoods.
  - c. *Bicycle* access shall be part of a 4' multi-use shoulder.
7. **Design Characteristics**
  - a. *Grades*
    1. Not less than one percent (1%) on tangents; nor more than four percent (4%). Grades of six percent (6%) may be considered for unique, short distances.

b. *Alignment*

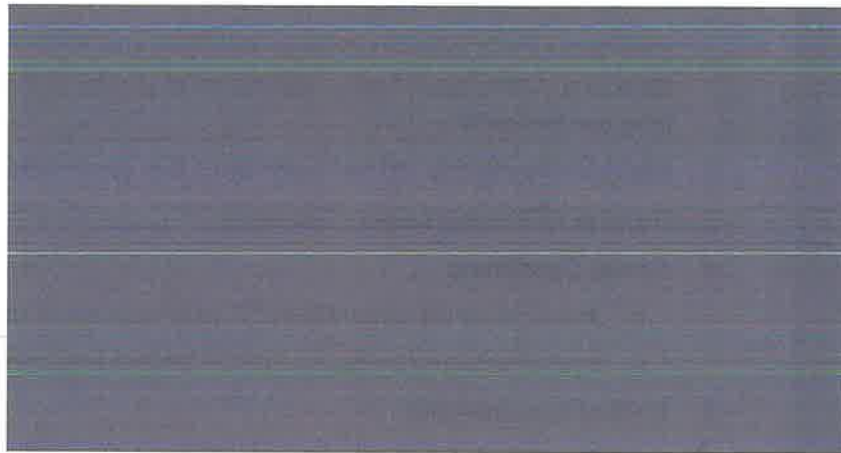
1. Horizontal – 765 foot minimum radius at centerline for standard crowned cross-slopes.
2. Vertical – minimum length equivalent to the K value times the algebraic difference approval of grades.

c. *Frequency of intersections*

1. Intersections along minor arterial streets shall be limited to as few as possible

Figure 12

**Minor Arterial**



**15.5**

**Standards for Collector Streets**

1. **Function** - Collector streets are designed to serve the local needs of the neighborhood and to provide direct access to non-residential, abutting properties. All traffic carried by collector streets should have an origin or a destination within the neighborhood.
2. **Right-of-Way Width** - 57 feet (no parking) to 67 feet (parking)
3. **Number of Moving Lanes**- Two lanes
4. **Access Conditions** - Direct access to residential properties is by way of curb cuts.
5. **Traffic Characteristics**
  - a. *On-street* parking is allowed on both sides of minor collector streets unless prohibited.
  - b. *Intersections* are at grade
6. **Planning Characteristics**
  - a. *Sidewalks* will be detached from vertical curbs
  - b. *Bicycle* travel can be accommodated with 14' shared lanes.
  - c. *No residential* frontage allowed on collectors with ADT greater than 2500.



## 7. Design Characteristics

### a. Grades

1. Not less than one percent (1%) on tangents; collector not more than ten percent (10%).

### b. Alignment

1. Horizontal curves – collector 335 foot minimum radius at centerline for standard crowned cross-slopes.
2. Vertical curves – A minimum length equivalent to the K value times the algebraic difference of approach grades.

### c. Frequency of intersections

1. Intersections along collector streets shall be limited to as few points as possible, while providing commercial access to abutting properties and connecting to local street system.

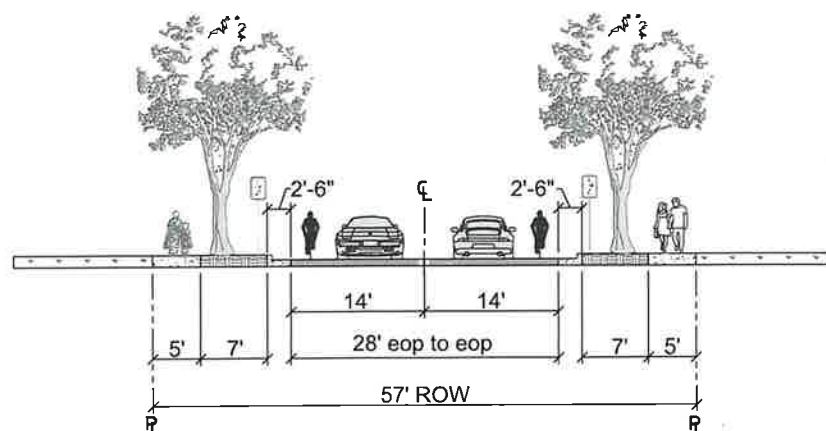
Figure 13

Collector (With Parking)



Figure 14

Collector (Without Parking)



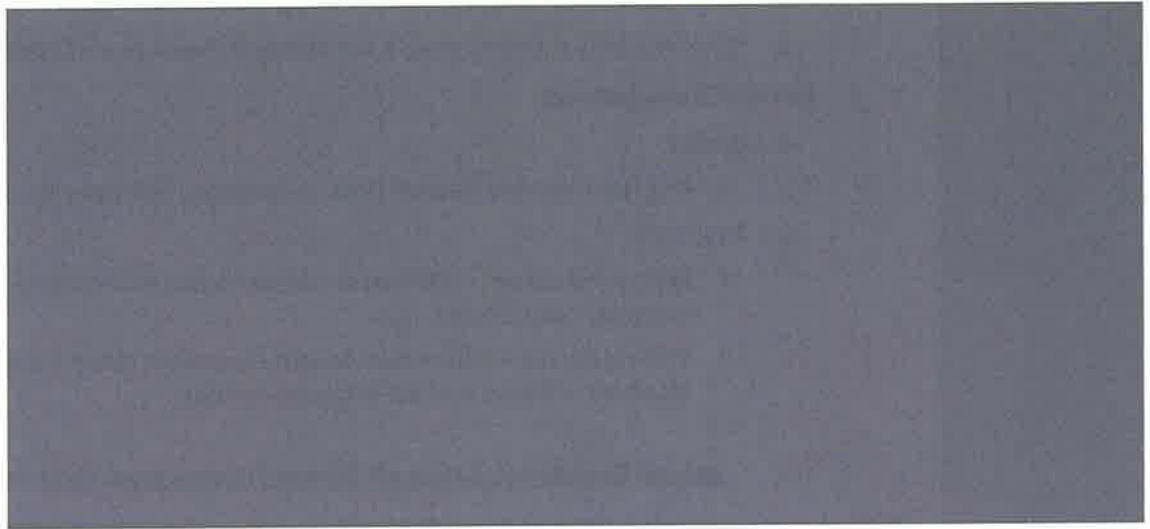
## 15.6

### Standards for Residential Streets

1. **Function** - Designed to serve the local needs of the neighborhood and to provide direct access to abutting residential properties. All traffic carried by residential streets should have an origin or a destination within the neighborhood.
2. **Right-of-Way Width** - 50 feet minimum plus 5 foot easements on each side of right-of-way for utilities and sidewalks.
3. **Number of Moving Lanes** - Two lanes
4. **Access Conditions** - Intersections are at grade with direct access to abutting properties by way of curb cuts or ramp-type curbing.
5. **Traffic Characteristics** - On-street parking is allowed on both sides of the street
6. **Planning Characteristics**
  - a. *Residential* streets should be designed to discourage through traffic and to encourage traffic speeds of 25 mph or less. These streets should not exceed 1200 feet in length and should include geometric features at intervals of 600' maximum. Examples of features include chokers (chicanes), traffic circles, median island/barriers, cul-de-sacs, and curvatures. Design criteria for these techniques are available in the Traffic Calming Design Manual.
  - b. *In subdivision* design, residential streets are discouraged from intersections with major and secondary arterial streets.
  - c. *Sidewalks* may be detached from or attached to the curb, depending upon the type of curb.
  - d. *Bicycle* travel can be accommodated in the travel lanes due to the low volume, low speed nature of this type of roadway.
7. **Design Characteristics**
  - a. *Grades*
    1. Not less than one percent (1%) on tangents; nor more than ten percent (10%)
  - b. *Alignment*
    1. Horizontal curves – 200 foot minimum radius at centerline for standard crowned cross-slopes.
    2. Vertical curves – a minimum length equal to the K times the algebraic difference of approach grades.
  - c. *Frequency of Intersections*
    1. Intersections along residential streets shall be allowed as needed to provide connections to other local streets and collector streets.

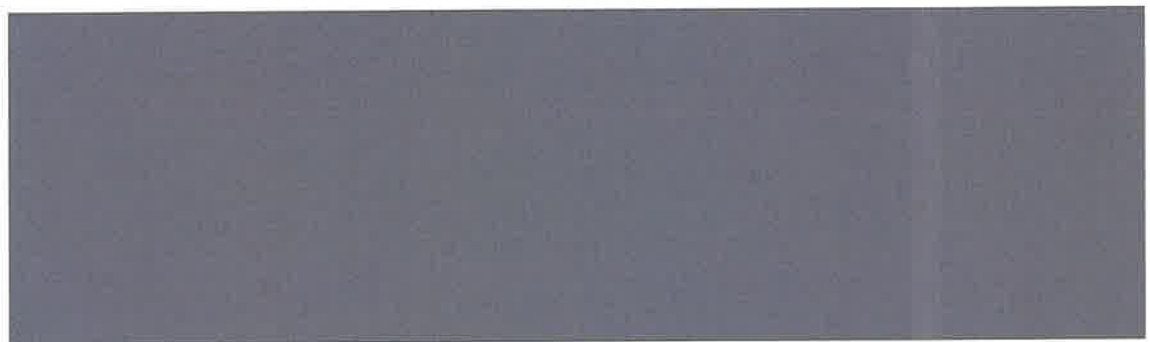
**Figure 15**

### Residential (Local) Streets (Detached Sidewalk)



**Figure 16**

### Residential (Local) Streets (Attached Sidewalk)



## 15.7

### Standards for Minor Residential Streets

1. **Function** - Designed to provide direct access to abutting single-family residential properties or cul-de-sacs having a length of no greater than 500 feet. A pavement mat of 24 feet is allowed on minor residential streets which contain no more than 20 single-family lots. Any other residential street having more than 20 single-family lots must install a 28 foot pavement mat.
2. **Right-of-Way** - 47 feet minimum plus 5 foot easements on each side of right-of-way for utilities and sidewalks.
3. **Number of Moving Lanes** - Two lanes
4. **Access Conditions** - Direct access to residential properties is by way of curb cuts or by ramp type curbs.
5. **Traffic Characteristics**
  - a. *On-street* parking is allowed on one side of the street
  - b. *Intersections* are at grade

**6. Planning Characteristics**

- a. *Minor residential streets should be designed as short loop or cul-de-sac streets only.*
- b. *Minor residential streets should not intersect major arterial streets.*

**7. Design Characteristics**

a. *Grades*

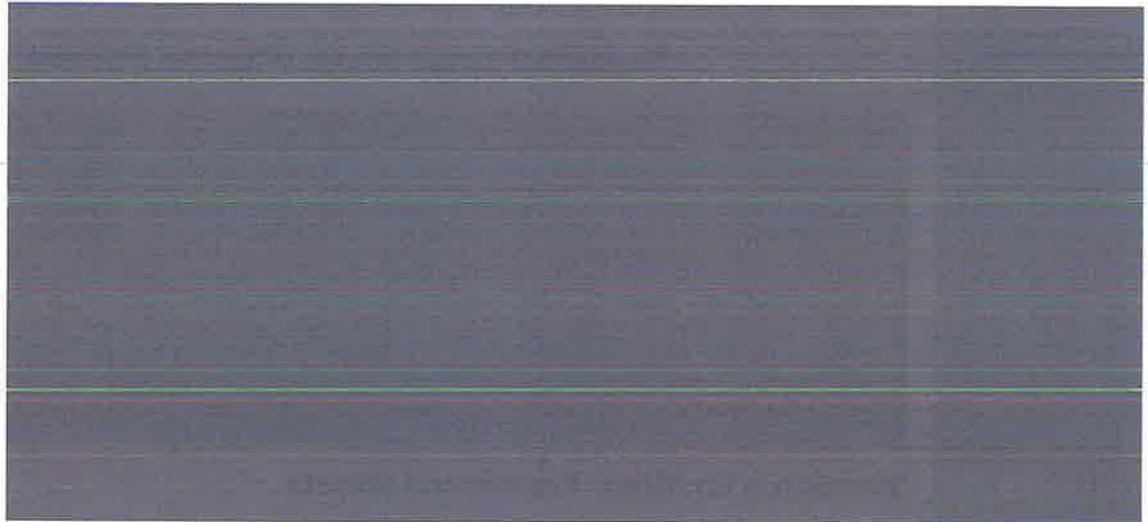
- 1. Not less than one percent (1%) on tangents; nor more than ten percent (10%)

b. *Alignment*

- 1. Horizontal curves – 200 foot minimum radius at centerline for standard crowned cross-slopes.
- 2. Vertical curves – A minimum length equivalent to the K value times the algebraic difference of the approach grades.

**Figure 17**

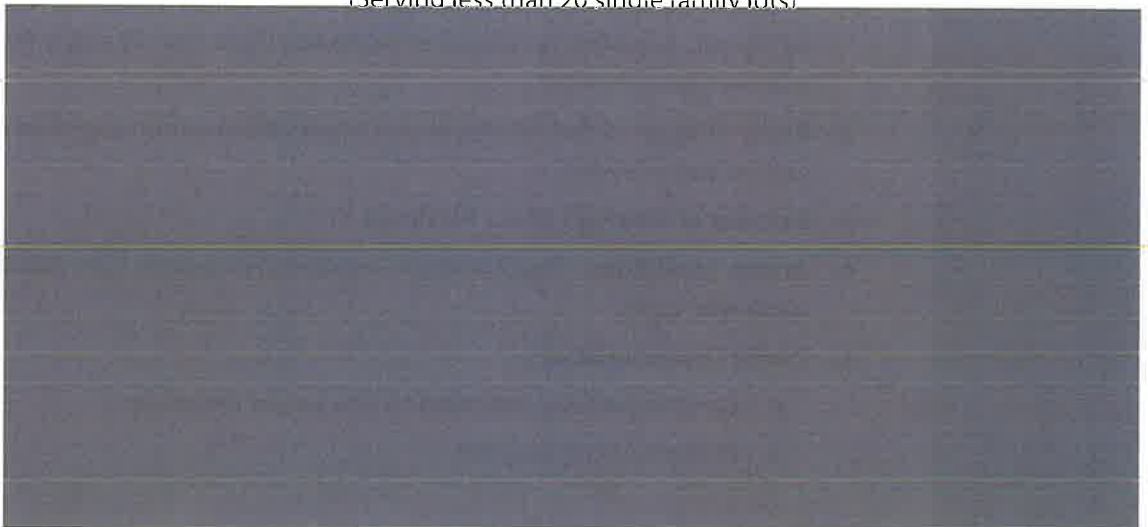
**Minor Residential (Local) Streets (Detached Sidewalk)**



**Figure 18**

**Minor Residential (Local) Streets (Attached Sidewalk)**

*(Serving less than 20 single family lots)*



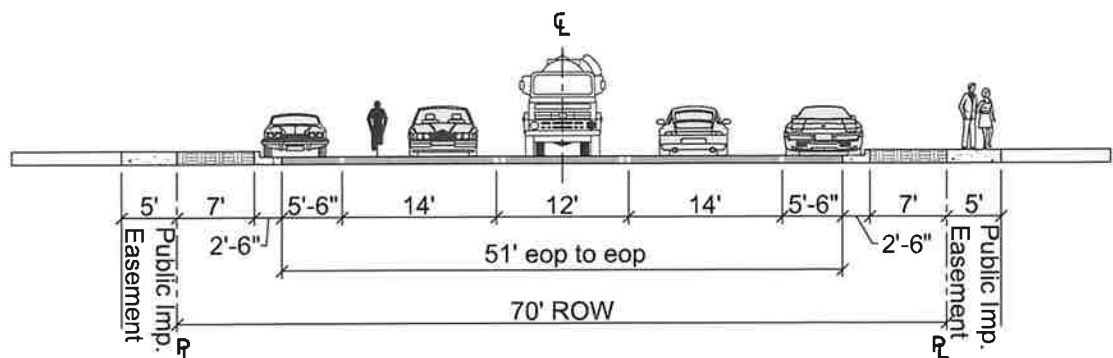
## 15.8

### Standards for Industrial Streets

1. **Function** - Industrial/ commercial streets are designed to serve facilities within industrial/ commercial areas and to connect such areas with major arterial and collector streets.
2. **Right-of-Way Width** - 70 feet (with two 5 foot easements)
3. **Number of Moving Lanes** - Two lanes to four lanes
4. **Access Conditions** - Direct access to abutting industrial/commercial properties is by way of curb cuts.
5. **Traffic Characteristics**
  - a. *On-street parking* may be permitted on both sides of the street
  - b. *Intersections* are at grade
6. **Planning Characteristics**
  - a. *Only local industrial/commercial traffic* should be encouraged on industrial streets
  - b. *Sidewalks* will be detached from the curb where required
7. **Design Characteristics**
  - a. *Grades*
    1. No less than one percent (1%) on tangents; nor more than eight percent (8%)
  - b. *Alignment*
    1. Horizontal – 335 foot minimum radius at centerline
    2. Vertical – A minimum length equivalent to the K value times the algebraic difference of the approach grades.
  - c. *Frequency of Intersections*
    1. Intersections along industrial/ commercial streets shall be limited to as few as possible, while connecting to the collector street system and providing access to local land uses.

Figure 19

#### Industrial Streets



## 15.9

### Standards for Alleys

1. **Function** - Designed to provide access to abutting property at rear lot lines.
2. **Right-of-Way Widths** - 20 foot (Residential)/ 25 foot (Commercial)
3. **Number of Moving Lanes** - Two lanes
4. **Access Conditions**
  - a. *Provide access to abutting property at rear of lots*
5. **Traffic Characteristics**
  - a. *Normally alleys shall intersect at perpendicular angles with streets*
  - b. *No parking shall be permitted*
6. **Planning Characteristics**
  - a. *Alleys shall be open at both ends*
  - b. *Normally alleys shall not intersect with collector streets or arterial streets.*
7. **Design Characteristics**
  - a. *Grade*
    1. Not less than one percent (1%) on tangents; nor more than ten percent (10%)
8. **Alley Drainage**

Alley surfaces may be designed to drain in three ways.

- Concrete V-shaped. The entire alley must be concrete. Concrete pans down the center of asphalt alleys are not permitted.
- Asphalt crowned with valley gutters on each side, or
- Asphalt cross-sloped with a valley gutter on the low side.

Flow spread in alleys shall be confined to the right-of-way at reasonable depths. Storm flows in alleys shall be limited to the flows generated from the rear of the lots adjoining the alley. Alley flows shall not cross the intersecting streets into another alley but shall be captured or diverted at the intersecting street. For more information about alley design reference is made to the design manuals for Traditional Neighborhood and Mixed Use Development.

Figure 20

### Residential Alleys

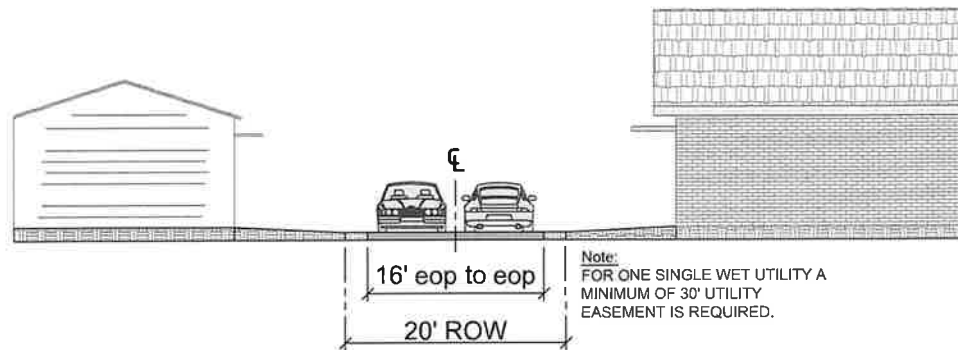
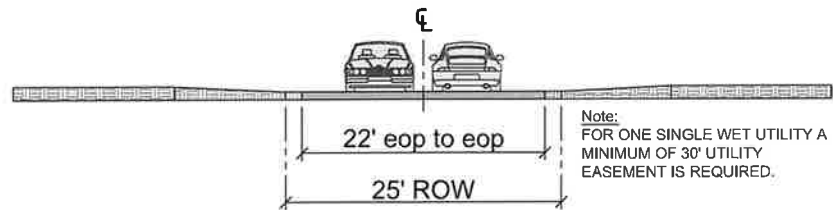


Figure 21

### Commercial Alleys



## Stakeholder Presentation Access Control Plan





provided internally from the existing access or new access to a roadway of lower functional classification.

**4. Relocation of Access when Alternative is Available**

All access to an expressway not meeting the minimum one-mile spacing requirement shall be closed in favor of an alternative access when an alternative is reasonably available in the opinion of the ECM Administrator.

**B. Rural and Urban Principal Arterial and Rural Minor Arterial Access Criteria**

**1. Spacing**

Spacing of roads accessing a principal arterial or rural minor arterial that will result in a full movement intersection shall be planned at one-half mile (one-quarter mile for rural minor arterials). Should the one-half mile spacing not be "viable or practical" for providing access to the adjacent land, a deviation may be considered and approved by the ECM Administrator. If a deviation is granted, only one additional full movement intersection will be permitted by the ECM Administrator. The Applicant shall have the burden of proof that no other "viable or practical" access is available. A deviation request should be supported by a traffic study or memorandum that provides information to assist the ECM Administrator in determining the proposed deviation minimizes negative safety and other operational impacts. If the development is at the intersection of two major corridors, the full movement access should be located on the lower functional classification roadway. The intersection shall only be approved if the intersection and roadway are shown to operate safely and efficiently with buildout design hour/peak hour projected traffic volumes. The intersection must also show a public benefit. An arterial progression through bandwidth percentage of 35 percent or greater must be achieved or the inclusion of a signal at the access must not degrade the existing signal progression. The intersection must not create any queuing or blocking of lane entries or access points. The intersection must be in a location such that any necessary turn, acceleration and deceleration lanes can be accommodated to maintain safe operations and capacity. The analysis should consider all potential future additional requirements for left turn or other exclusive phasing at a signal for which the need is created by traffic generated by land uses on both sides of the roadway.

**2. Topographic and Other Limitations**

Where topography or other existing conditions make the required spacing inappropriate or unfeasible, location of the access shall be determined with consideration given to topography, established property ownerships, unique physical limitations, pre-existing historical land use patterns, and physical design constraints, with every attempt to achieve an access spacing of one-half mile. The final location shall serve as

many properties as possible to reduce the need for additional direct access to the principal arterial or rural minor arterial. In selecting locations for full movement intersections, preference shall be given to roads that meet, or may be reasonably expected to meet, signal warrants in the future.

**3. Access and Lot Division**

No additional access right shall accrue and no additional access shall be provided when splitting or dividing existing lots of land. When an alternative is reasonably available in the opinion of the ECM Administrator, all access to the newly created properties shall be provided internally from the existing access or new access to a roadway of lower functional classification.

**C. Urban Minor Arterial Access Criteria**

Spacing of roads accessing an urban minor arterial that will result in a full movement intersection shall be planned at one-quarter mile. However, one parcel access shall be granted to each existing lot, if it does not create safety or operational problems. The parcel access will provide for right turns only. The access may allow for left turns in (three-quarters movement) if the addition of left turns will improve the operation at an adjacent full movement intersection and meet appropriate design standards.

**D. Collector Access Standards**

Collector roadways shall intersect another roadway (centerline to centerline) in accordance with the standards in Section 2.3.7. On minor collector roadways, the closest local roadway intersection to an arterial roadway shall be 330 feet (right-of-way line of arterial to centerline of local roadway). On major collector roadways, the closest local roadway intersection to an arterial roadway shall be 660 feet (right-of-way line of arterial to centerline of local roadway). Single-family residence access to major collector roadways is not permitted (even though existing conditions show otherwise).

**E. Rural and Urban Local Roadways**

Roads shall not intersect urban local roadways closer than 175 feet from each other (centerline to centerline) and shall not intersect a rural local roadway closer than 330 feet from each other. On an urban local roadway, the closest intersection to a collector roadway shall be at least 200 feet (centerline to centerline). To an arterial roadway, the closest intersection shall be 330 feet (arterial right-of-way line to local roadway centerline).

**2.2.6 Soils Investigations**

**A. General**

Soil investigations are conducted to help in preparing designs of roadways and other public facilities. The soil investigation report provides detailed information

# 16.0

## Table of Traffic Engineering Design Standards

### Traffic Engineering Design Standards (Freeways, Expressways and Arterials)

Design Element	Functional Classification				
	Freeway	Expressway	Principal Arterial Type 2 (6 lane)	Principal Arterial Type 1 (4 lane)	Minor Arterial
<b>Speeds (1)</b>	65	55	45	45	40
<b>Design ADT</b>	85,000-100,000	60,000-85,000	25,000-60,000	10,000-25,000	5,000-25,000
<b>Trip Length</b>	Over 5 miles	Over 5 miles	1-2 miles	1-2 miles	Over 1 mile
<b>Corridor ROW Width</b>	332'-420'	210'	142'	107'	90' w/ (2) 5' easements
<b>Roadway Width (pavement mat)</b>	Var. Width	2-50' pavement mat	2-40' pavement mat	2-28' pavement mat	69'
<b># of Lanes</b>	6-8	4-6	6	4	4
<b>Lane Widths</b>	12'	12'	11'	11'	11'
<b>Shoulder Width</b>	12'	10'	4'	4'	4'
<b>Median</b>	Var. Width	Raised 28'	Raised 28'	Raised 17'	Raised 17'
<b>Sidewalk Requirement (placement)</b>	N/A	N/A	Detached 6'	Detached 6'	Detached 6'
<b>Bicycle Accommodation</b>	N/A	N/A	6' Multi-Use Shoulder	6' Multi-Use Shoulder	5' Multi-Use Shoulder
<b>Tree lawn Width</b>	N/A	N/A	7'	7'	7'
<b>Parking</b>	No	No	No	No	No
<b>Access</b>	Full Control	Full Control	Full Control	Full Control	Full Control
<b>Design Vehicle</b>	WB 67	WB 67	WB 67	WB 67	WB 50
<b>Signalized Intersection Frequency</b>	N/A	1 mile	½ mile	½ mile	½ mile
<b>Unsignalized Intersection Frequency</b>	1 mile	N/A	¼ mile	¼ mile	600'
<b>Vertical Alignment</b>	Refer to Vertical Curve Design in AASHTO Geometric Design of Highways and Streets				
<b>Horizontal Alignment Radius</b>	N/A	N/A	1045'	1040'	765'
<b>Grade (min-max)</b>	1%-4%	1%-4%	1%-4%	1%-4%	1%-4%
<b>Intersection Grade</b>	Grade Separ.	1% min	1% min	1% min	1% min
<b>Intersection Sight Distance</b>	775'	665'	500'	500'	445'
<b>Stopping Sight Distance (2)</b>	730'	570'	360'	360'	305'

**Traffic Engineering Design Standards**  
**(Collector, Residential [Local], Public Alley, and Industrial)**

Design Element					
	Collector	Residential (Local)	Minor Residential (Local)	Public Alley	Industrial
Speeds (1)	30	25	25	15	30
Design ADT	1,500-5,000	300-1,500	50-300	50-300	<10,000
Trip Length	1 mile	Local	Local	Local	Truck Local
Maximum Uninterrupted Facility Length	¼ mile	600'	300'	Adjacent Street Length	1 mile
Corridor ROW Width	57' (no parking) 67' (parking)	50' w/ (2) 5' easements	47' w/ (2) 5' easements	20' Residential 25' Commercial	70' w/ (2) 5' easements
Roadway Width (pavement mat)	28' (no parking) 38' (parking)	30'	24' (<21 Lots) 28' (>20 Lots)	16' Residential 22' Commercial	51'
# of Lanes	2	2	2	2	3
Lane Widths	14' w/ shared bike	9'	N/A	N/A	14' w/shared bike w/12' ctl
Shoulder Width	N/A	N/A	N/A	N/A	N/A
Median	N/A	N/A	N/A	N/A	N/A
Sidewalk Requirement (placement)	Detached 5'	Attached 6' vert. curb/ Detached 5' others	Attached 6' vert. curb/ Detached 5' others	N/A	Detached 5'
Bicycle Accommodation	On street w/ shared lane	On street w/ shared lane	On street w/ shared lane		On street w/ shared lane
Tree lawn Width	7'	7'-6"	7'	N/A	7'
Parking	Allowed	Two Sides	One-side parking only	No	Two sides
Access	Partial Control	Partial Control	Partial Control	N/A	Partial Control
Design Vehicle	WB 40	SU 30	SU 30	N/A	WB 67
Signalized Intersection Frequency	N/A	N/A	N/A	N/A	½ mile
Un-signalized Intersection Frequency	600'	300' max	300' max	½ adjacent street length	600'

Design Element					
	Collector	Residential (Local)	Minor Residential (Local)	Public Alley	Industrial
Vertical Alignment	Refer to Vertical Curve Design in AASHTO Geometric Design of Highways and Streets				
Horizontal Alignment Radius	335'	200'	200'	85'	335'
Grade (min-max)	1%-10%	1%-10%	1%-10%	1%-10%	1%-8%
Intersection Grade	1%-3%	1%-4%	1%-4%	1%-4%	1%-3%
Intersection Sight Distance	335'	280'	280'	170'	335'
Stopping Sight Distance (2)	200'	155'	155'	80'	200'

*Note: Alternate design standards may apply when Traditional Neighborhood, Mixed Use, Hillside, or Low-Impact Development are used.*

*(1) Speed refers to the anticipated posted speed. The design speed is 5 mph greater than the posted speed.*

*(2) For level terrain only.*

## Stakeholder Coordination 11/10/2020 Developers





Presentation of Preferred Alignment, Cross Section and Access Management Strategies  
**Briargate Parkway - Stapleton Road Corridor Study**



**Developer Stakeholders Briefing Meeting**

2:00 PM – 3:00 PM | November 10, 2020  
El Paso County – Pikes Peak Rural Transportation Authority Project

D-41





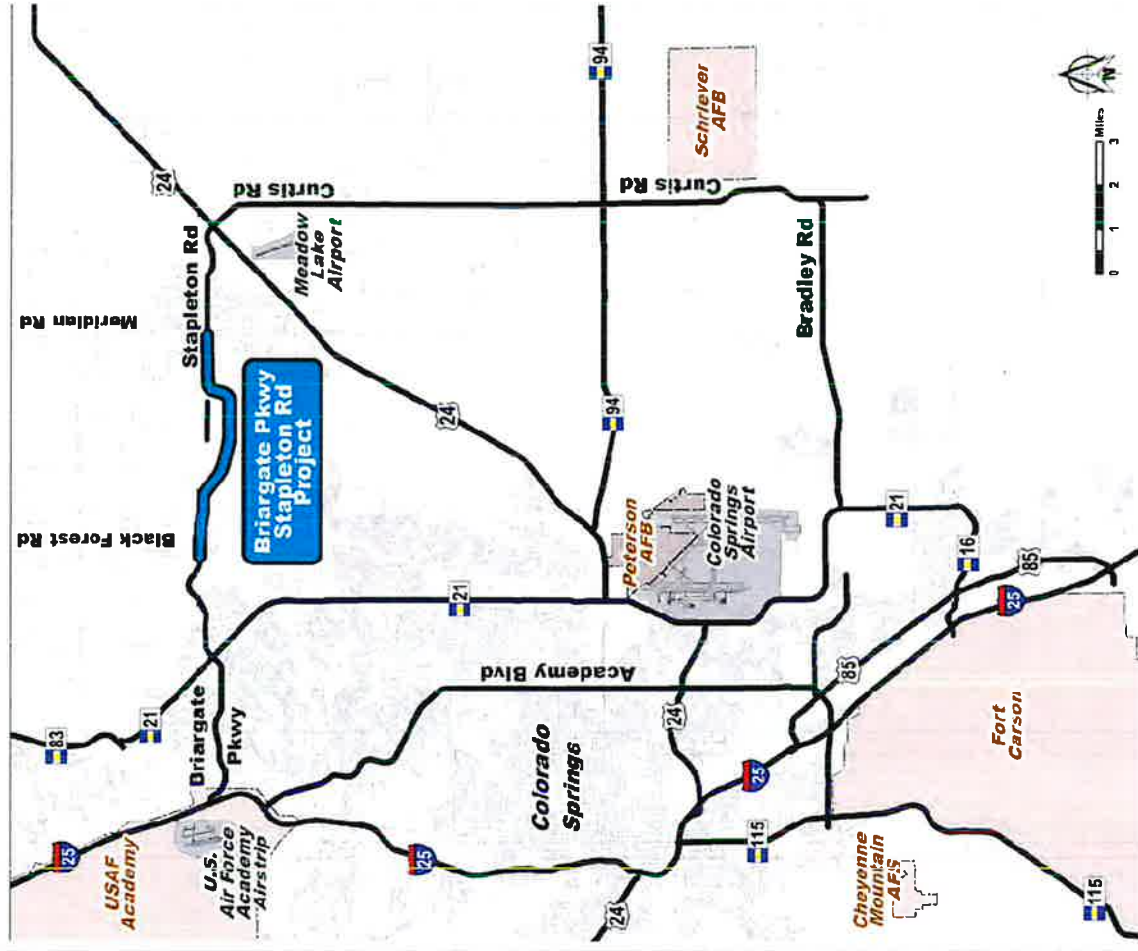
# Agenda

- ◆ Corridor Alignment
- ◆ Roadway Cross Section
- ◆ Access Management
- ◆ Multimodal Facilities
- ◆ Utilities

# Context

- ◆ The Briargate-Stapleton Project Corridor is part of regional **principal arterial** route that provides north-south connectivity as well as connections on the north and south to I-25.
- ◆ The corridor is **identified as a key facility** in PPACG's Regional Transportation Plan, El Paso County's Major Transportation Corridor Plan and the City of Colorado Springs current Intermodal Transportation Plan.

BRIARGATE PARKWAY - STAPLETON ROAD CORRIDOR STUDY



# Study Deliverables

- ◆ Corridor Preservation Plan
- ◆ Pre-NEPA Environmental Overview
- ◆ Access Management Plan

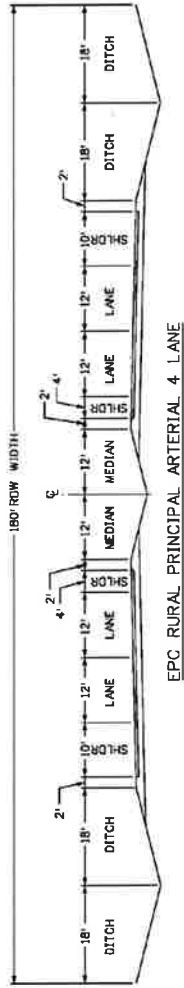
# Corridor Alignment



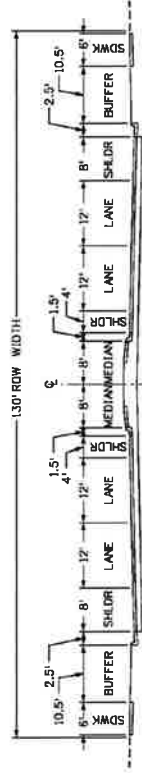
BRIARGATE PARKWAY - STAPLETON ROAD CORRIDOR STUDY

# Roadway Cross Section

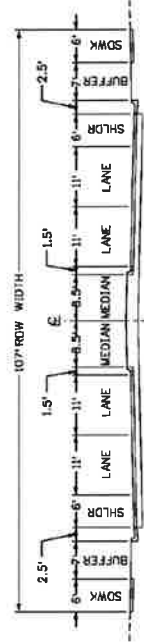
## STANDARD 4-LANE CROSS SECTIONS



EPC RURAL PRINCIPAL ARTERIAL 4 LANE



EPC URBAN PRINCIPAL ARTERIAL 4 LANE

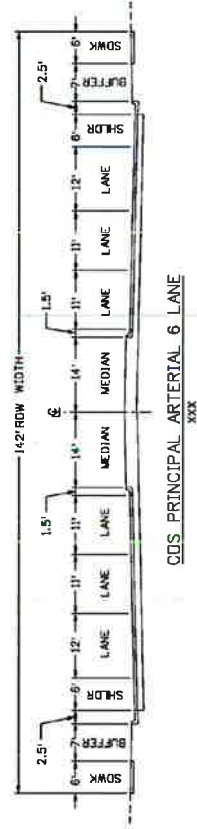
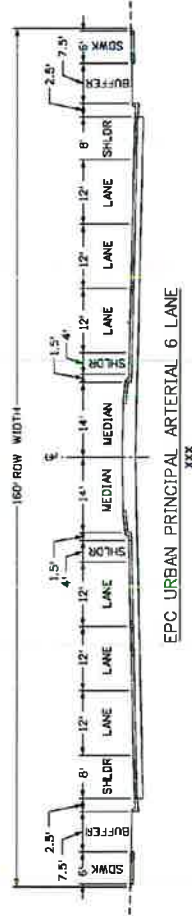
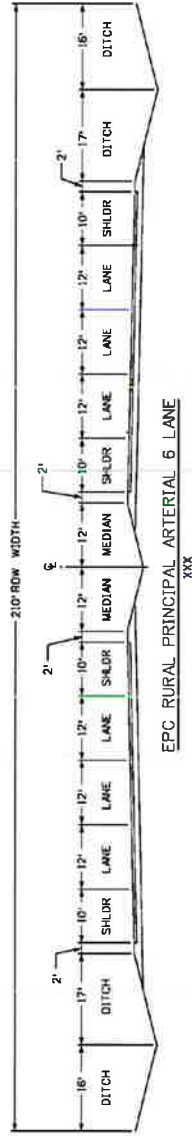


COS PRINCIPAL ARTERIAL 4 LANE

BRIARGATE PARKWAY - STAPLETON ROAD CORRIDOR STUDY

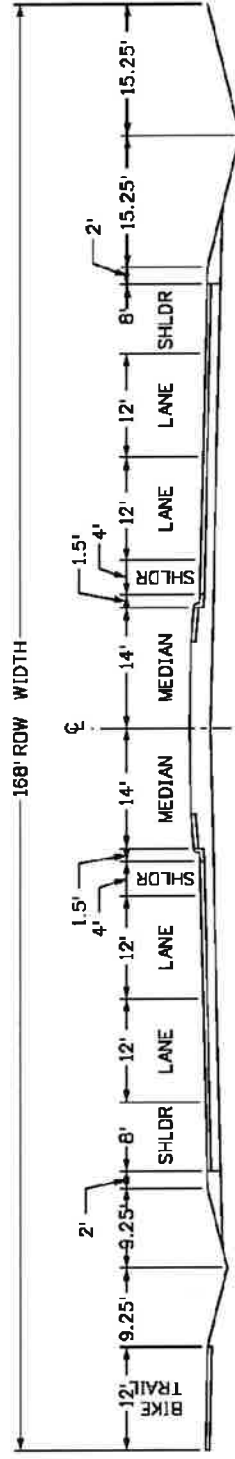
# Roadway Cross Section

## STANDARD 6-LANE CROSS SECTIONS

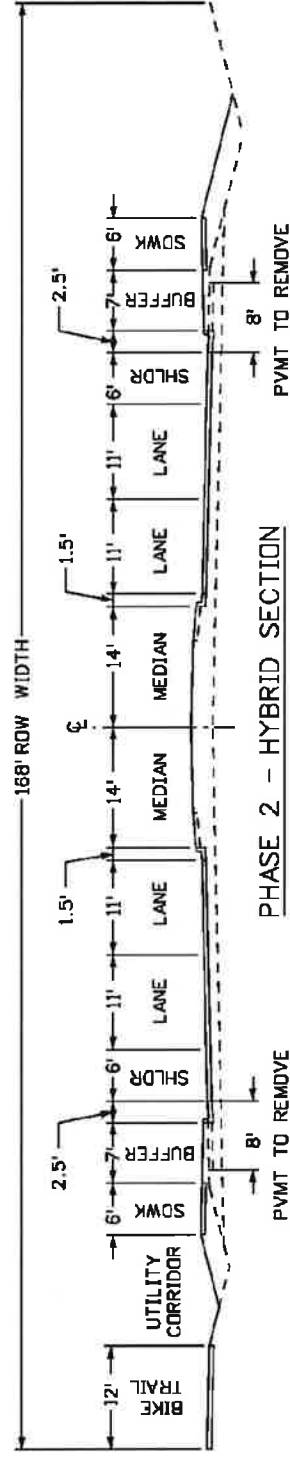


# Roadway Cross Section

## HYBRID CROSS SECTION & PHASING



PHASE 1 - HYBRID SECTION



PHASE 2 - HYBRID SECTION



# Access Management

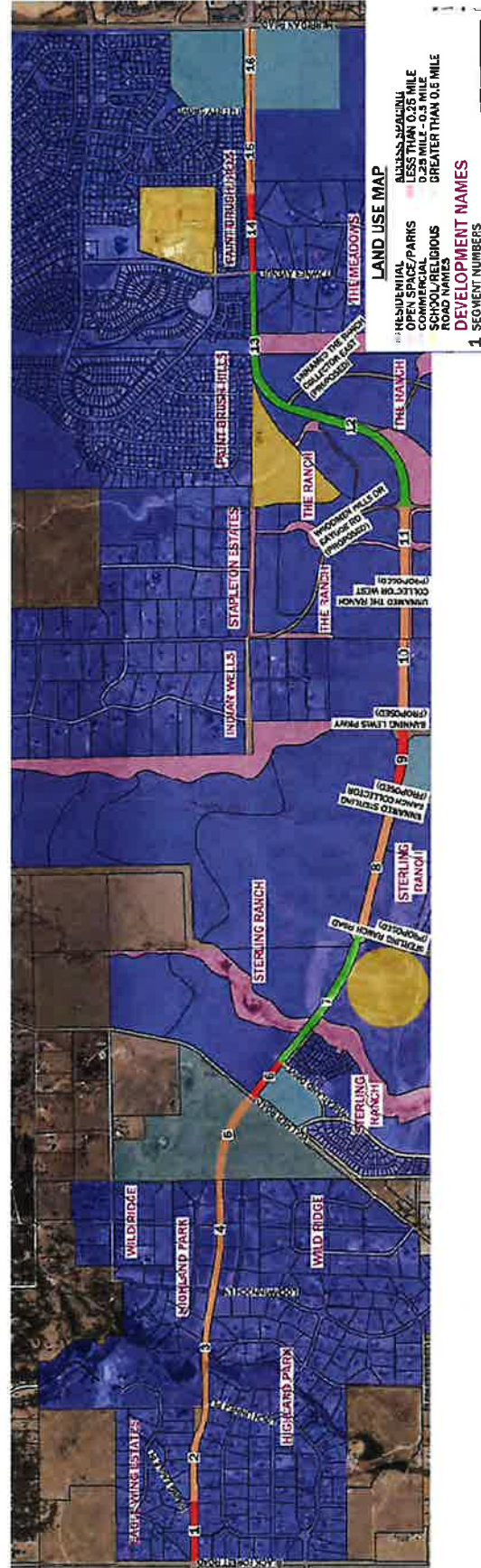
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## 4-LANE PRINCIPAL ARTERIAL ACCESS CRITERIA

- ◆ El Paso County
  - Spacing of 1/2 mile between intersections (2640')
- ◆ City of Colorado Springs
  - Spacing of 1/2 mile between signalized intersections (2640')
  - Spacing of 1/4 mile between unsignalized intersections (1320')
  - Same criteria for 6 lanes



# Access Management



Segment Number	Western Road	Eastern Road	Approximate Spacing
1	Black Forest Rd	Rising Eagle Place	1075' (0.20mi)
2	Rising Eagle Place	Loch Linnch Place	1700' (0.32mi)
3	Loch Linnch Place	Loch Linnch Lane	2000' (0.36mi)
4	Loch Linnch Lane	Proposed Commercial Access	1920' (0.36mi)
5	Proposed Commercial Access	Vollmer Road	1530' (0.25mi)
6	Vollmer Road	Wheatland Drive	750' (0.14mi)
7	Wheatland Drive (proposed)	Sterling Ranch Road (proposed)	2700' (0.51mi)
8	Sterling Ranch Road (proposed)	Sterling Ranch Collector (proposed)	2450' (0.46mi)
9	Sterling Ranch Collector (proposed)	Banning Lewis Parkway (proposed)	1100' (0.21 mi)

Segment Number	Western Road	Eastern Road	Approximate Spacing
10	Banning Lewis Parkway (proposed)	The Ranch Collector West (proposed)	2300' (0.44 mi)
11	The Ranch Collector West (proposed)	Woodmen Hills Drive/Raygor Road (proposed)	1530' (0.29 mi)
12	Woodmen Hills Drive/Raygor Road (proposed)	The Ranch Collector East (proposed)	2900' (0.55 mi)
13	The Ranch Collector East (proposed)	Towner Avenue	3570' (0.67 mi)
14	Towner Avenue	Scenic Brush Drive	1340' (0.25 mi)
15	Scenic Brush Drive	Liberty Grove Drive	1450' (0.27 mi)
16	Liberty Grove Drive	Meridian Road	1450' (0.27 mi)

BRIARGATE PARKWAY - STAPLETON ROAD CORRIDOR STUDY

# Access Management Strategies

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- ◆ Right-In/Right-Out Access Restrictions
  - Rising Eagle (Segments 1 and 2)
  - Wheatland Drive (Segments 5 and 6)
  - Sterling Ranch Collector (Segments 8 and 9)
  - Scenic Brush Drive (Segments 13 and 14)
- ◆ Implement 2020 MTCP/Plat Notes
  - Highland Park:
    - There shall be no vehicular access to Briargate Parkway. El Paso County shall be contacted prior to establishment of any driveway.
  - The Ranch:
    - The developer will construct the extension of Stapleton/Briargate through the property to a 4-lane principal arterial functional classification.
    - Until approved by the County, all proposed access locations, road locations, widths and alignments, roundabout locations and design shown on this sketch plan are conceptual and subject to change. Final location and design will be determined through the subdivision process.
    - The eastern intersection of the proposed urban residential collector loop and Stapleton Drive/Briargate Parkway may be signalized or constructed as a modern roundabout

## Multimodal Facilities

- ◆ Large multipurpose shoulders
- ◆ Separate bike path
- ◆ Separated sidewalks
- ◆ EPC Parks interested in grade-separated pedestrian crossing (where County regional trail crosses just east of Sterling Ranch)

# Utilities

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- ◆ Utilities Corridor outside of roadway footprint
- ◆ Identified by CSU as preferred utilities corridor
- ◆ Timing is of interest to CSU

# Thank You

Maureen Paz de Araujo, FAICP CTP CEP  
Senior Transportation Planner  
Wilson & Company

[maureen.pazdearaujo@wilsonco.com](mailto:maureen.pazdearaujo@wilsonco.com)

Project Website: <https://www.briargate-stapleton.com>

Prepared by



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Colorado Springs, CO 80919  
719-520-5800

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## AFFIDAVIT OF PUBLICATION

STATE OF COLORADO  
COUNTY OF El Paso

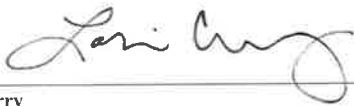
I, Lorre Cosgrove, being first duly sworn, deposes and says that she is the Legal Sales Representative of The Colorado Springs Gazette, LLC., a corporation, the publishers of a daily/weekly public newspapers, which is printed and published daily/weekly in whole in the County of El Paso, and the State of Colorado, and which is called Colorado Springs Gazette; that a notice of which the annexed is an exact copy, cut from said newspaper, was published in the regular and entire editions of said newspaper 1 time(s) to wit 09/22/2023

That said newspaper has been published continuously and uninterruptedly in said County of El Paso for a period of at least six consecutive months next prior to the first issue thereof containing this notice; that said newspaper has a general circulation and that it has been admitted to the United States mails as second-class matter under the provisions of the Act of March 3, 1879 and any amendment thereof, and is a newspaper duly qualified for the printing of legal notices and advertisement within the meaning of the laws of the State of Colorado.

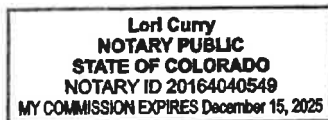


Lorre Cosgrove  
Sales Center Agent

Subscribed and sworn to me this 09/25/2023, at said City of Colorado Springs, El Paso County, Colorado.  
My commission expires December 15, 2025.



Lori Curry  
Notary Public  
The Gazette



Document Authentication Number  
20164040549-185304

LEGAL NOTICE
<b>MASTER PLAN</b> <b>BRIARGATE / STAPLETON CORRIDOR PRESERVATION PLAN</b> <b>AND ACCESS CONTROL PLAN</b>
NOTICE IS HEREBY GIVEN that on October 5, 2023, at 9:00 A.M. in the second floor Hearing Room of the Pikes Peak Regional Development Center located at 7040 International Circle, Colorado Springs, Colorado, 80910, or at the time or place to which the hearing may be adjourned, a public hearing will be held by the Planning Commission of the County of El Paso, State of Colorado on the application described below.
NOTICE IS HEREBY GIVEN that on November 2, 2023, at 9:00 A.M. in the second floor Hearing Room of the Pikes Peak Regional Development Center located at 7040 International Circle, Colorado Springs, Colorado, 80910, or at the time or place to which the hearing may be adjourned, a public hearing will be held by the Planning Commission of the County of El Paso, State of Colorado on the application described below.
NOTICE IS HEREBY GIVEN that on November 30, 2023, at 9:00 A.M. in the Centennial Hall Auditorium of 200 S. Cascade Avenue, Colorado Springs, Colorado, 80903, or at the time or place to which the hearing may be adjourned, a date to set the public hearing on the application will be determined by the Board of County Commissioners of the County of El Paso, State of Colorado.
The proposed service plan and related documents may be viewed online at the following web address: <a href="https://ppcdplanreview.com">https://ppcdplanreview.com</a> , searching file number 109231.
The El Paso County Department of Public Works requests adoption of the Briargate Parkway/Stapleton Road Corridor Preservation Plan and Access Control Plan ("Plan") into the Year El Paso County Master Plan. With adoption, this Plan will become the principal Plan for further planning and development of the Briargate Parkway/Stapleton Road corridor within unincorporated El Paso County. The Plan area begins at Black Forest Road, which is the eastern boundary of the Wolf Ranch subdivision and coincides with the eastern boundary of the City of Colorado Springs. The terminus of the Plan area is along Stapleton Road at Meridian Road. Call Commissioner District 11C.
Dated at Colorado Springs, Colorado, this 20th day of September 2023.
THE BOARD OF COUNTY COMMISSIONERS OF EL PASO COUNTY, COLORADO
BY /s/ Cami Bremner Chair
Published in The Gazette September 22, 2023.



Birgates-Stackton Stakeholder Input Tracking: August - September 2022

Stakeholder email addresses										Method of Contact				V/N/R (Remove Add to Project Mailing List?)	Phone	Address	Notes
File No.	Received Date	Received Day/Time	Response Date	City/Town/ Community	Comment Category	Comment	Stakeholder Proposed Solution	Interactive Map / Community Remarks	Mobile Contact Form Email	Direct Email or Email Response	Phone	First Name	Last Name	Email			
2	08/02/22	Friday 10:00 AM	N/A		Support - 1 Zoning/Growth/ROW - 2 Water/Light/Air/Drainage - 3 Traffic Impacts - 4 Transportation - 5 Safety - 6 Multimodal - 7 General - 8 Access/Traffic - 9 Other - 0	0 Off topic: Solving Wix website optimization services.						Oliver	Jahson	info@birgates-stackton.com	N		
5	08/12/22	Friday 4:13 PM	08/16/23			0 Off topic: Solving Wix website optimization services.						Kyle	Stark	kyle.stark@buckin.com	N		
6	08/12/22	Friday 6:20 PM	08/16/23			0 Off topic: Solving Wix website optimization services.						David	Furney		Y		
7	08/12/22	Friday 7:08 PM	08/16/23			0 Off topic: Solving Wix website optimization services.						Ch		whit0703@yahoo.com	N		
8	08/12/22	Friday 9:43 PM	08/16/23			0 Off topic: Solving Wix website optimization services.											
9		Saturday 7:41 AM	08/16/23			0 OK. Did not want to be added to mailing list.											
10	08/13/22	Saturday 8:09 AM	08/16/23			1 I am completely in favor of this project. (Especially with the addition of a bike path and pedestrian walkway separate from the roadway, this would be an amazing addition to our area. In there any sort of bike frame when this could become a reality.)						Justin	Shank	justin.shank@gmail.com	Y		
13	08/13/22	Saturday 5:30 PM	08/16/23			1 I will be on the 2nd road (next to Woodmen) that travels westward from 125 to 140th. I am currently in the process of proposing we use an easement road, and have many of us currently use it. Expect the same amount, or more on Birgates. Napoleon. Please, please, don't do this like the extension of Dublin where it's only 2 lanes... or heaven forbid, Mark's belief. Clearly this community is too big NOT to plan for additional growth and usage.						Marcia	Hurt	marciahurt125@gmail.com	Y		
14a	08/13/22	Saturday 11:12 PM	08/16/23			1 Hi, we as homeowners in Sterling Ranch are excited to have Birgates connect to our area. We have to drive around (either north or south) just to get to into town, school, and work. This will expedite so much for us and are excited for this to take action. Thanks!						Margan	Lundberg	mrg_lundberg@gmail.com	Y		
14b	08/14/22	Sunday 11:28 PM	08/16/23			1 Hi, we as homeowners in Sterling Ranch are excited to have Birgates connect to our area. We have to drive around (either north or south) just to get to into town, school, and work. This will expedite so much for us and are excited for this to take action. Thanks!						Kyle		kallie.kg@hotmail.com	Y		
17	08/14/22	Sunday 6:30 AM	08/16/23			1 Hi, we as homeowners in Sterling Ranch are excited to have Birgates connect to our area. We have to drive around (either north or south) just to get to into town, school, and work. This will expedite so much for us and are excited for this to take action. Thanks!						David	Collard	collardd2@aol.com	Y		
18	08/14/22	Sunday 6:34 AM	08/16/23			1 Hi, we as homeowners in Sterling Ranch are excited to have Birgates connect to our area. We have to drive around (either north or south) just to get to into town, school, and work. This will expedite so much for us and are excited for this to take action. Thanks!						Katie	Luge	kallie.kg@hotmail.com	Y		
19	08/14/22	Sunday 7:11 AM	08/16/23	Palom		1 Hi, we as homeowners in Sterling Ranch are excited to have Birgates connect to our area. We have to drive around (either north or south) just to get to into town, school, and work. This will expedite so much for us and are excited for this to take action. Thanks!						Lori	Foster	crabty1462@aol.com	Y		
21	08/14/22	Sunday 7:39 AM	08/16/23			2 Resolving: I HIGHLY disapprove of having commercial buildings along the proposed corridor off Volmer Rd! We live out here to be AWAY from the city and it's crime. I realize a ROAD is needed to alleviate the traffic congestion with all the overbuilding in the area. The infrastructure can't support all the homes being put in there!											
22a	08/14/22	Sunday 8:18 AM	08/16/23			2 Commercial development constraints: As in Germany, the US needs to take a As in Germany, the US needs to take a lesson learned and work more to contain commercial growth into urban and more so rural areas.						Terrill	McCall	Terrillmcca@hotmail.com	Y		







SR#	DATE/TIME	NAME	ADDRESS	PHONE	EMAIL	STATUS	REMARKS	DATE/TIME	NAME	ADDRESS	PHONE	EMAIL	STATUS	REMARKS
380	08/27/23	Monday 2:15 PM					2) I have concerns that my street will become a major street be cut through the neighborhood. We moved here to get away from heavy traffic that was behind our previous home. I have already seen some cars cut through to get from Volmer to Black Forest rd. We are already disappointed that there are going to be homes built on small lots right up to our development that are 2.5 acre lots. It is possible for our community of Highland Park to become a great community.							
40	08/22/22	Monday 3:00 PM					2) See email in Word Doc for details. Opposed to the project. Stated issues related to the following: 1. ROW 2. Speeds 3. Lights 4. Noise 5. Traffic congestion 6. Environmental impacts, safety concerns and traffic pressure on the road. 7. Sidewalks and bike lanes on a roadway with a speed limit of 45mph, no traffic circles, and the level of traffic flow is dangerous; 8. The need to communicate with the Highland Park community directly and transparently, informing the community of updates, timelines, public meetings, anything else associated with this project.							
57	08/31/22	Wednesday 5:57 PM					2) Hello! Our lot in Highland Park backs to the proposed future Brigade Parkway so information about this project is very important to us. We are wondering particularly at this stage if there will be a wall constructed along the property lines of those lots that butt up against the road. The markers indicating the roadway in between our lot and the road are not visible and it is not clear if there will be enough room for a lane inauguration and any kind of greenpace. Can you provide more insight on what this will look like? Thank you.							
58	09/08/22	Thursday 7:41 AM					3) Hello, I just built a house on the Highland Park development and my house will be very close to this project. We were looking at the plans and don't like the idea of the stop light. It needs to be a round about instead of the stop light. Also a noise control wall.							
59	09/08/22	Thursday 4:32 PM					3) Our home is being built at 7781 Ramoth Moor Way. We access our home via Lodwinch Lane. We would beg the planners to opt for a roundabout at the Bringle/Lodwinch Lane intersection instead of a traffic signal light. The noise generated at a traffic light is greater due to the ever present idling cars with noisy engines. A traffic light will not necessarily slow traffic, where a roundabout will. A roundabout will slow traffic, but the intersection is far greater than traffic moving through a roundabout. A traffic light will ruin the dark side and quiet, peaceful atmosphere of our beautiful subdivision. PLEASE, PLEASE opt for roundabouts through Highland Park subdivision.							





[illegible]

	Date	Time	Location	Requester Name	Requester Email	Status	Comments	Response Date	Response Time	Response Status	Response Comments	Response Author	Response Email
77	06/08/23	Friday 4:30 PM	Highland Park			No comment. Mailing list request only.							nraadreyes@gmail.com
44	06/09/23	Tuesday 8:30 AM				No comment. Mailing list request only.							nancy.duo@gmail.com
47	06/09/23	Tuesday 7:35 PM	781 Rannoch Moor Way Highland Park 3			No comment. Mailing list request only.							jessiekrine@gmail.com
61b	06/09/23	Friday 2:23 PM				No comment. Mailing list request only.							rtzye@hotmail.com
63	06/09/23	Wednesday 9:41 PM				No comment. Mailing list request only.							cathbowen1@gmail.com
68	06/09/23	Friday 9:37 PM				No comment. Mailing list request only.							rick@psychiatry.com
45	06/09/23	Tuesday 4:34 PM				No comment. Mailing list request only.							connec42@gmail.com
53a	06/09/23	Monday 8:05 AM				No comment. Mailing list request only.							dobley1@yahoo.com
51b	06/09/23	Friday 11:58 AM	Northeast corner of Blackforest and Burgate			No comment. Mailing list request only.							jafer1776@gmail.com
1	06/09/23	Monday 10:41 AM	Pantherbrook Hills Community			No comment. Mailing list request only.							rachanne@gmail.com
34b	06/09/23	Thursday 8:48 AM				No comment. Mailing list request only.							mudnack185@gmail.com
50	06/09/23	Monday 3:24 PM				No comment. Mailing list request only.							deryl@graydon.com
74	06/09/23	Monday 4:10 PM				No comment. Mailing list request only.							nancy.duo@gmail.com
55	06/09/23	Tuesday 9:10 PM				No comment. Mailing list request only.							nancy.duo@gmail.com
63	06/09/23	Wednesday 5:44 PM				No comment. Mailing list request only.							corneyw578@gmail.com
64	06/09/23	Friday 9:37 PM				No comment. Mailing list request only.							lyyad1m11@yahoo.com
1	06/09/23	Monday 10:41 AM	Pantherbrook Hills Community			No comment. Mailing list request only.							songdoginuzer@gmail.com
4	06/09/23	Friday 4:12 PM				No comment. Mailing list request only.							pamgmac.com
11	06/09/23	Sunday 9:36 AM				No comment. Mailing list request only.							jillandave@yahoo.com
12	06/09/23	Saturday 2:03 PM	Hightown Park North Union			Please keep me posted on your development since it seems to have some major impact on our subdivision. Mailing list request only.							tony.jicko@att.net
24	06/09/23	Sunday 8:41 AM				No comment. Mailing list request only.							katyale@comcast.net
28	06/09/23	Sunday 4:41 PM	Union Station neighborhood			No comment. Mailing list request only.							yannaweth@gmail.com
46	06/09/23	Wednesday 2:17 PM				No comment. Mailing list request only.							bjbjmcm@aol.com
48a	06/09/23	Friday 3:55 PM				No comment. Mailing list request only.							esthanduse@comcast.net
49b	06/09/23	Thursday 9:41 PM				No comment. Mailing list request only.							techhanduse@comcast.net

53a	08/28/22	Monday 8:35 PM	06/17/23						✓	Greg	Martin	gregmartin946@gmail.com	Y			
53b	08/29/22	Monday 8:39 PM	06/17/23						✓	Greg	Martin	gregmartin946@gmail.com	Y			
54	08/29/22	Monday 8:34 PM	06/17/23						✓	Steve	Under	idore.kindy@hotmail.com	Y			
55	08/30/22	Tuesday 7:59 PM	06/17/23						✓	Uma	Mac'hutch	u155uct@propietx.com net				
60	09/13/22	Tuesday 12:52 PM	06/17/23	East of The Ranch subdivision					✓	Kevin	McGrath	planned2@yahoo.com	N	715-688-8913	10953 Carewell Drive, Falcon, CO	
62a	09/13/22	Tuesday 2:30 PM	06/17/23						✓	Linda	Summers	summers.101@netnet.net	Y			
67	09/16/22	Friday 9:13 PM	06/17/23						✓	Thomas	Barlett	checkup_touch@cloud.com	Y			
69	09/17/22	Monday 12:12 PM	06/17/23						✓	Gary	Schneider	gtschneider@gmail.com	Y			
70	09/20/22	Tuesday 12:42 PM	06/17/23						✓	Brooks	Geyer	Brooks@geyer@yahoo.com	Y			
71	09/25/22	Sunday 7:06 PM	06/17/23						✓							
73	10/16/22	Sunday 8:17 PM	06/17/23						✓							
76	04/09/23	Saturday 11:56 AM	06/17/23						✓	Arturo	Alon	lu.alon@live.com	Y			
78	04/08/23		06/17/23						✓	Alysa	Waskewicz	alwaskewicz@gmail.com	Y			
80	02/20/23	6:27 PM	06/17/23						✓	Robert		rockymhopp@gmail.com	Y			
76	04/22/23	8:28 PM	06/17/23						✓	Roger	Rosly	roger.rosly10@yahoo.com	Y			
78			06/17/23						✓	Alysa	Waskewicz	alwaskewicz@gmail.com	Y			
80	05/10/23	7:41 AM	06/17/23						✓	M	Harding	mharding1981@gmail.com	Y			
83	04/21/23	8:29 PM	06/17/23						✓	Dave	Sparks	dsparks@free@gmail.com	Y			
84	04/21/23	8:49 PM	06/17/23						✓	Roger	Rosly	roger.rosly10@yahoo.com	Y			
85	04/27/23	6:22 PM	06/17/23						✓	Dave	Collins	conductor@aol.com	Y			
86	04/20/23	3:25 PM	06/17/23						✓	Jason		oelectricty97@yahoo.com	Y			
87	06/10/23	7:41 AM	06/17/23						✓	M	Harding	mharding1981@gmail.com				

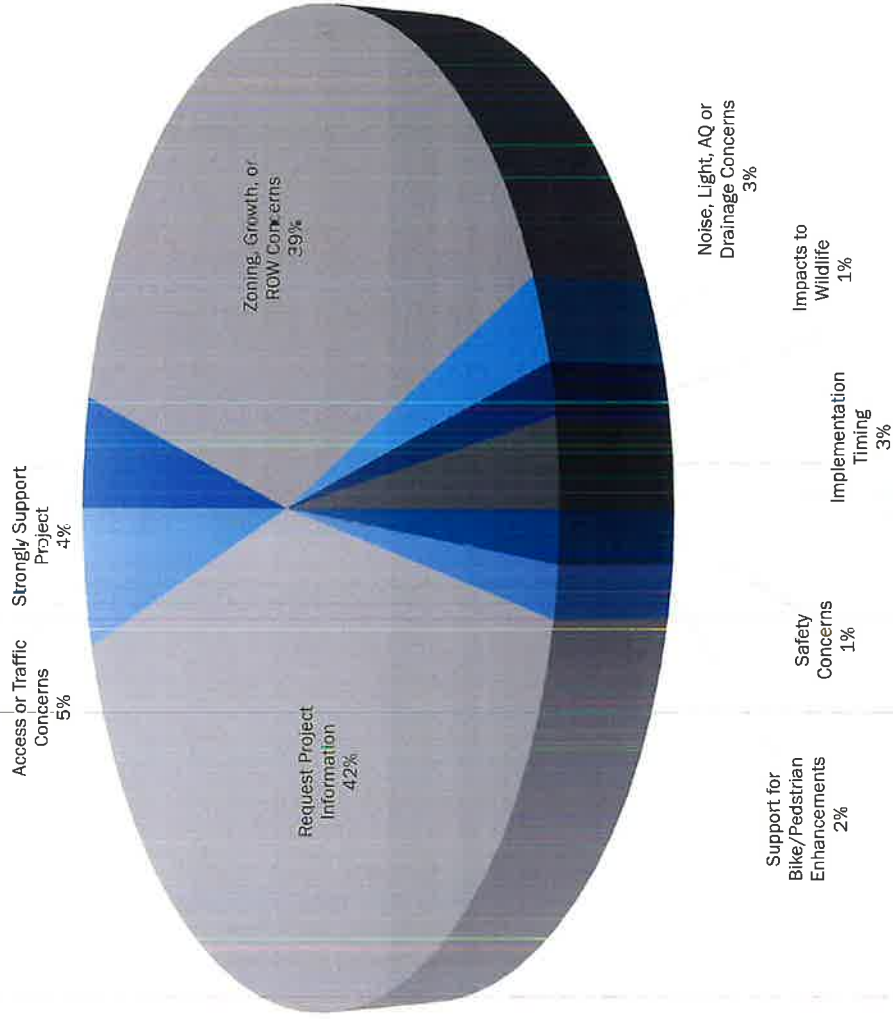








Strongly Support Project	8
Zoning, Growth, or ROW Concerns	77
Noise, Light, AQ or Drainage Concerns	5
Impacts to Wildlife	3
Implementation Timing	5
Safety Concerns	3
Support for Bike/Pedestrian Enhancements	3
Request Project Information	82
Access or Traffic Concerns	10
	196
Other - 0	6



**Briargate-Stapleton Planning Study Public Comment Email Responses Round 4 (August –September 2022)****GENERIC RESPONSE TEMPLATE/CONTENT)**

**Response Text** (red text added to explain delayed response to comments):

Dear First Last Name:

Thank you for connecting with El Paso County through our Briargate-Stapleton Project for Mobility website, by using our interactive map, and/or via email to share your roadway safety concerns and improvement preferences, including XYZ.

Since the 30-day public comment period closed in September 2022, the project encountered some delay, but is now ready to progress in earnest. It is anticipated that both the Corridor Preservation Plan and the Access Control Plan will be presented to the Planning Commission and the Board of County Commissioners in mid to late summer for review and adoption, respectively. Presentation to the Planning Commission will occur at two regular meetings, the first as a public hearing. The County will advertise the specific dates of these meetings and meeting notices will also be posted on the project website.

Your input, which has been recorded for review, will play a valuable role in shaping the conceptual design of the roadway, designed to improve access and enhance safety for the entire community and provide long-term benefits for El Paso County.

Stay up to date by accessing our comprehensive website: <https://www.briargate-stapleton.com/>.

We appreciate your continued participation!

Maureen Paz de Araujo, FAICP CTP CEP  
Senior Transportation Planner

**Response Text for Mailing List Request Only:**

Thank you for connecting with us through our Briargate-Stapleton Project for Mobility website, by using our interactive map, and/or via email. As requested, you have been added to the project mailing list.

Stay up to date by accessing our comprehensive website: <https://www.briargate-stapleton.com/>.

We appreciate your continued participation!

Maureen Paz de Araujo, FAICP CTP CEP  
Senior Transportation Planner



**File/Record Number: 01**

**From:** Justin Shonk <[reply-to+fd965c997f9c@crm.wix.com](mailto:reply-to+fd965c997f9c@crm.wix.com)>

**Sent:** Monday, July 11, 2022 10:41 AM

**To:** Paz de Araujo, Maureen A. <[Maureen.PazdeAraujo@wilsonco.com](mailto:Maureen.PazdeAraujo@wilsonco.com)>; [briargatestapleton@gmail.com](mailto:briargatestapleton@gmail.com)

**Subject:** [briargate-stapleton] Contacts Form - new submission

**Justin Shonk** just submitted your form: Contacts Form  
on [briargate-stapleton](#)

### Message Details:

Name: Justin Shonk

Email: [shonkatay@gmail.com](mailto:shonkatay@gmail.com)

Subject: Comments

Message: As a resident of the Paintbrush Hills Community that will be directly affected by this, I am completely in favor of this project.

Especially with the addition of a bike path and pedestrian walkway separate from the roadway, this would be an amazing addition to our area. Is there any sort of time frame when this could become a reality? I look forward to hearing from you. Thanks!

Add me to the project mailing list.: Checked

**Response 06/19/23**

Dear Justin Shonk:

Thank you for connecting with El Paso County through our Briargate-Stapleton Project for Mobility website, by using our interactive map, and/or via email to share your support of the overall project and specific support for the addition of a bike path and pedestrian walkway.

The timeline for construction has not yet been identified. At this point only the study has been funded. The study will set the alignment, typical roadway section, and access control plan for the corridor that will be used for right-of-way preservation through the County's development review process.

Since the 30-day public comment period closed in September 2022, the project encountered some delay, but is now ready to progress in earnest. It is anticipated that both the Corridor Preservation Plan and the Access Control Plan will be presented to the Planning Commission and the Board of County Commissioners in mid to late summer for review and adoption, respectively. Presentation to the Planning Commission will occur at two regular meetings, the first as a public hearing. The County will advertise the specific dates of these meetings and meeting notices will also be posted on the project website.

Your input, which has been recorded for review, plays a valuable role in shaping the conceptual design of the roadway, designed to improve access and enhance safety for the entire community and provide long-term benefits for El Paso County.

As requested, you have been added to the project mailing list.

Stay up to date by accessing our comprehensive website: <https://www.briargate-stapleton.com/>.

We appreciate your continued participation!

Maureen Paz de Araujo, FAICP CTP CEP  
Senior Transportation Planner

**Tracked: Y**

**File/Record Number:** 03 N/A SPAM Solicitation

**From:** Oliver Johnson <reply-to+6b676755ca0f@crm.wix.com>  
**Sent:** Tuesday, August 2, 2022 10:47 AM  
**To:** Paz de Araujo, Maureen <Maureen.PazdeAraujo@wilsonco.com>;  
briargatestapleton@gmail.com  
**Subject:** [briargate-stapleton] Contacts Form - new submission

Oliver Johnson just submitted your form: Contacts Form  
on [briargate-stapleton](#)

### Message Details:

**Name:** Oliver Johnson

**Email:** [info@briargate-stapleton.com](mailto:info@briargate-stapleton.com) **Subject:** Wix

**Message:** My name is Oliver and I represent a company that employs 46 experts in the design and optimization of websites for Wix. There are several errors in your website code that cause most of the content to not even be indexed by Google which results in low traffic. Your website was created in the Wix editor so it's relatively easy to fix all the errors. If you want to know which elements of your website need to be changed to achieve significantly higher rankings in Google, fill out the form below: [320.webextools.com](https://320.webextools.com) In response, you will receive a completely free report, from which you will learn what needs to be changed in order for your website to gain a much higher position in Google. Best regards, Oliver Johnson.

**Add me to the project mailing list:** Unchecked

**Response:** 00/00/22 N/A

**Tracked:** Y

**File/Record Number:** 03 N/A Suspicious Email – No info requested

**From:** cc@g.bbb <reply-to+432b873bcc81@crm.wix.com>

**Sent:** Friday, August 12, 2022 4:04 PM

**To:** Paz de Araujo, Maureen <Maureen.PazdeAraujo@wilsonco.com>;  
briargatestapleton@gmail.com

**Subject:** [briargate-stapleton] Contacts Form - new submission

A site visitor just submitted your form: Contacts Form  
on [briargate-stapleton](#)

### Message Details:

**Name:**–

**Email:** [cc@g.bbb](#) **Subject:** Cc **Message:** Cc

**Add me to the project mailing list:** ☐ Unchecked

**Response** 00/00/22 N/A

**Tracked:** Y

**File/Record Number: 04**

**From:** Paula Whitehead <reply-to+95e103a89c9b@crm.wix.com>  
**Sent:** Friday, August 12, 2022, 4:12 PM  
**To:** Paz de Araujo, Maureen <Maureen.PazdeAraujo@wilsonco.com>;  
 briargatestapleton@gmail.com  
**Subject:** [briargate-stapleton] Contacts Form - new submission

**Paula Whitehead** just submitted your form: Contacts Form  
 on [briargate-stapleton](#)

**Message Details:**

Name: Paula Whitehead Email: [petn@mac.com](mailto:petn@mac.com)

Subject: Briargate-Stapleton Project for Mobility

Message: Please provide any available details concerning this project. Thank you.

Add me to the project mailing list.: Checked

**Response 06/19/23**

Dear Paula Whitehead:

Thank you for connecting with El Paso County through our Briargate-Stapleton Project for Mobility website, by using our interactive map, and/or via email to share your roadway safety concerns and improvement preferences. Your input, which has been recorded for review, plays a valuable role in shaping the conceptual design of the roadway, designed to improve access and enhance safety for the entire community and provide long-term benefits for El Paso County.

All of the available details concerning this project may be found by accessing our comprehensive website: <https://www.briargate-stapleton.com/>. As requested, you have been added to the project mailing list.

Since the 30-day public comment period closed in September 2022, the project encountered some delay, but is now ready to progress in earnest. It is anticipated that both the Corridor Preservation Plan and the Access Control Plan will be presented to the Planning Commission and the Board of County Commissioners in mid to late summer for review and adoption, respectively. Presentation to the Planning Commission will occur at two regular meetings, the first as a public hearing. The County will advertise the specific dates of these meetings and meeting notices will also be posted on the project website.

Stay up to date by accessing our comprehensive website: <https://www.briargate-stapleton.com/>.

We appreciate your continued participation!

Maureen Paz de Araujo, FAICP CTP CEP  
 Senior Transportation Planner

**Tracked: Y**



**File/Record Number: 05**

**From:** Larry Bell <reply-to+5249b7509257@crm.wix.com>

**Sent:** Friday, August 12, 2022, 4:13 PM

**To:** Paz de Araujo, Maureen <Maureen.PazdeAraujo@wilsonco.com>;

briargatestapleton@gmail.com

**Subject:** [briargate-stapleton] Contacts Form - new submission

**Larry Bell** just submitted your form: Contacts Form  
on [briargate-stapleton](#)

**Message Details:**

Name: Larry Bell

Email: [lbell719@gmail.com](mailto:lbell719@gmail.com)

Subject: -

Message: -

Add me to the project mailing list.: Checked

**Response 06/19/23**

Dear Larry Bell:

Thank you for connecting with us through our Briargate-Stapleton Project for Mobility website, by using our interactive map, and/or via email. As requested, you have been added to the project mailing list.

Since the 30-day public comment period closed in September 2022, the project encountered some delay, but is now ready to progress in earnest. It is anticipated that both the Corridor Preservation Plan and the Access Control Plan will be presented to the Planning Commission and the Board of County Commissioners in mid to late summer for review and adoption, respectively. Presentation to the Planning Commission will occur at two regular meetings, the first as a public hearing. The County will advertise the specific dates of these meetings and meeting notices will also be posted on the project website.

Stay up to date by accessing our comprehensive website: <https://www.briargate-stapleton.com/>.

We appreciate your continued participation!

Maureen Paz de Araujo, FAICP CTP CEP  
Senior Transportation Planner

**Tracked: Y**

**File/Record Number: 06**

**From:** Kale <reply-to+174ffab6d4e2@crm.wix.com>  
**Sent:** Friday, August 12, 2022, 6:24 PM  
**To:** Paz de Araujo, Maureen <Maureen.PazdeAraujo@wilsonco.com>;  
briargatestapleton@gmail.com  
**Subject:** [briargate stapleton] Contacts Form new submission

Kale just submitted your form: Contacts Form  
on [briargate-stapleton](#)

**Message Details:**

Name: Kale

Email: [kale89@gmail.com](mailto:kale89@gmail.com) Subject: -

Message: -

Add me to the project mailing list.: Unchecked

**Response 06/19/23**

Dear Kale:

Thank you for connecting with us through our Briargate-Stapleton Project for Mobility website, by using our interactive map, and/or via email. As requested, you have been removed from the project mailing list.

Since the 30-day public comment period closed in September 2022, the project encountered some delay, but is now ready to progress in earnest. It is anticipated that both the Corridor Preservation Plan and the Access Control Plan will be presented to the Planning Commission and the Board of County Commissioners in mid to late summer for review and adoption, respectively. Presentation to the Planning Commission will occur at two regular meetings, the first as a public hearing. The County will advertise the specific dates of these meetings and meeting notices will also be posted on the project website.

Stay up to date by accessing our comprehensive website: <https://www.briargate-stapleton.com/>.

We appreciate your continued participation!

Maureen Paz de Araujo, FAICP CTP CEP  
Senior Transportation Planner

**Tracked:**

**File/Record Number: 07**

**From:** David Buck <reply-to+253450887e05@crm.wix.com>  
**Sent:** Friday, August 12, 2022, 7:08 PM  
**To:** Paz de Araujo, Maureen <Maureen.PazdeAraujo@wilsonco.com>;  
~~[briargatestapleton@gmail.com](mailto:briargatestapleton@gmail.com)~~  
**Subject:** [briargate-stapleton] Contacts Form - new submission

---

David Buck just submitted your form: Contacts Form  
on [briargate-stapleton](#)

### Message Details:

Name: David Buck

Email: [buckidave@icloud.com](mailto:buckidave@icloud.com)

Subject: Briargate-Stapleton Project Message: -

Add me to the project mailing list.: Checked

### Response 06/19/23

Dear David Buck:

Thank you for connecting with us through our Briargate-Stapleton Project for Mobility website, by using our interactive map, and/or via email. As requested, you have been added to the project mailing list.

Since the 30-day public comment period closed in September 2022, the project encountered some delay, but is now ready to progress in earnest. It is anticipated that both the Corridor Preservation Plan and the Access Control Plan will be presented to the Planning Commission and the Board of County Commissioners in mid to late summer for review and adoption, respectively. Presentation to the Planning Commission will occur at two regular meetings, the first as a public hearing. The County will advertise the specific dates of these meetings and meeting notices will also be posted on the project website.

Stay up to date by accessing our comprehensive website: <https://www.briargate-stapleton.com/>.

We appreciate your continued participation!

Maureen Paz de Araujo, FAICP CTP CEP  
Senior Transportation Planner

**Tracked: Y**

**File/Record Number: 08**

**From:** Robert Tillman <reply-to+e39da8380fd6@crm.wix.com>  
**Sent:** Friday, August 12, 2022, 9:43 PM  
**To:** Paz de Araujo, Maureen <Maureen.PazdeAraujo@wilsonco.com>;  
briargatestapleton@gmail.com  
**Subject:** [briargate-stapleton] Contacts Form - new submission

**Robert Tillman** just submitted your form: Contacts Form  
on [briargate-stapleton](#)

**Message Details:**

Name: Robert Tillman

Email: [bobtillmanp fwd@gmail.com](mailto:bobtillmanp fwd@gmail.com)

Subject: Briargate pRkWY Message: -

Add me to the project mailing list.: Checked

**Response 06/19/23**

Dear Robert Tillman:

Thank you for connecting with us through our Briargate-Stapleton Project for Mobility website, by using our interactive map, and/or via email. As requested, you have been added to the project mailing list.

Since the 30-day public comment period closed in September 2022, the project the project encountered some delay, but is now ready to progress in earnest. It is anticipated that both the Corridor Preservation Plan and the Access Control Plan will be presented to the Planning Commission and the Board of County Commissioners in mid to late summer for review and adoption, respectively. Presentation to the Planning Commission will occur at two regular meetings, the first as a public hearing. The County will advertise the specific dates of these meetings and meeting notices will also be posted on the project website.

Stay up to date by accessing our comprehensive website: <https://www.briargate-stapleton.com/>.

We appreciate your continued participation!

Maureen Paz de Araujo, FAICP CTP CEP  
Senior Transportation Planner

**Tracked: Y**

**File/Record Number: 09**

**From:** ZAKALUK STEPHEN <reply-to+f788f5839807@crm.wix.com>  
**Sent:** Saturday, August 13, 2022, 7:41 AM  
**To:** Paz de Araujo, Maureen <Maureen.PazdeAraujo@wilsonco.com>;  
 briargatestapleton@gmail.com  
**Subject:** [briargate-stapleton] Contacts Form - new submission

**ZAKALUK STEPHEN** just submitted your form: Contacts Form  
 on [briargate-stapleton](#)

**Message Details:**

Name: ZAKALUK STEPHEN

Email: [szakaluk@icloud.com](mailto:szakaluk@icloud.com)

Subject: The Ranch Message: -

Add me to the project mailing list.: Unchecked

**Response 06/19/23**

Dear Stephen Zalaluk:

Thank you for connecting with us through our Briargate-Stapleton Project for Mobility website, by using our interactive map, and/or via email. As requested, you have been removed from the project mailing list.

Since the 30-day public comment period closed in September 2022, the project encountered some delay, but is now ready to progress in earnest. It is anticipated that both the Corridor Preservation Plan and the Access Control Plan will be presented to the Planning Commission and the Board of County Commissioners in mid to late summer for review and adoption, respectively. Presentation to the Planning Commission will occur at two regular meetings, the first as a public hearing. The County will advertise the specific dates of these meetings and meeting notices will also be posted on the project website.

Stay up to date by accessing our comprehensive website: <https://www.briargate-stapleton.com/>.

We appreciate your continued participation!

Maureen Paz de Araujo, FAICP CTP CEP  
 Senior Transportation Planner

**Tracked: Y**

**File/Record Number: 10**

**From:** Nancy <reply-to+ab5b217a59da@crm.wix.com>  
**Sent:** Saturday, August 13, 2022, 8:09 AM  
**To:** Paz de Araujo, Maureen <Maureen.PazdeAraujo@wilsonco.com>;  
briargatestapleton@gmail.com  
**Subject:** [briargate-stapleton] Contacts Form - new submission

Nancy just submitted your form: Contacts Form  
on [briargate-stapleton](#)

**Message Details:**

Name: Nancy

Email: [horseylady@msn.com](mailto:horseylady@msn.com)

Subject: Stapleton briargate Message: -

Add me to the project mailing list.: Unchecked

**Response 06/19/23**

Dear Nancy:

Thank you for connecting with us through our Briargate-Stapleton Project for Mobility website, by using our interactive map, and/or via email. As requested, you have been removed from the project mailing list.

Since the 30-day public comment period closed in September 2022, the project encountered some delay, but is now ready to progress in earnest. It is anticipated that both the Corridor Preservation Plan and the Access Control Plan will be presented to the Planning Commission and the Board of County Commissioners in mid to late summer for review and adoption, respectively. Presentation to the Planning Commission will occur at two regular meetings, the first as a public hearing. The County will advertise the specific dates of these meetings and meeting notices will also be posted on the project website.

Stay up to date by accessing our comprehensive website: <https://www.briargate-stapleton.com/>.

We appreciate your continued participation!

Maureen Paz de Araujo, FAICP CTP CEP  
Senior Transportation Planner

**Tracked: Y**

**File/Record Number: 11**

**From:** Jill Khaliqi <reply-to+4d69a945cc11@crm.wix.com>  
**Sent:** Saturday, August 13, 2022, 9:36 AM  
**To:** Paz de Araujo, Maureen <Maureen.PazdeAraujo@wilsonco.com>; briargatestapleton@gmail.com  
**Subject:** [briargate-stapleton] Contacts Form - new submission  
 Jill Khaliqi just submitted your form: Contacts Form  
 on [briargate-stapleton](#)

**Message Details:**

Name: Jill Khaliqi

Email: [jillanddave@yahoo.com](mailto:jillanddave@yahoo.com)

Subject: Website broken

Message: I'm trying to get to the project overview. Every option gives me this "contact" page and nothing else.  
 Add me to the project mailing list.: Checked

**Response 06/19/23**

Dear Jill Khaliqi:

Thank you for connecting with El Paso County through our Briargate-Stapleton Project for Mobility website, by using our interactive map, and/or via email to share your roadway safety concerns and improvement preferences. We are sorry to hear you had issues accessing the Project Overview.

Since the 30-day public comment period closed in September 2022, the project encountered some delay, but is now ready to progress in earnest. It is anticipated that both the Corridor Preservation Plan and the Access Control Plan will be presented to the Planning Commission and the Board of County Commissioners in mid to late summer for review and adoption, respectively. Presentation to the Planning Commission will occur at two regular meetings, the first as a public hearing. The County will advertise the specific dates of these meetings and meeting notices will also be posted on the project website.

Please access our comprehensive website at: <https://www.briargate-stapleton.com/>. On the homepage, scroll down below the Welcome section to find the following Project Overview:

"As shown by the map, the Briargate Parkway–Stapleton Road (in some locations referred to as Stapleton Drive) corridor is an integral part of a larger transportation corridor system providing connectivity with I-25 to the north and south of the greater Colorado Springs area. Stapleton Road continues easterly from this project area to connect with US 24 and with Judge Orr Road and Curtis Road to the east of the City. Curtis Road provides connection to Bradley Road and ultimately to I-25 south of Colorado Springs. The corridor study section under consideration as part of this project is mostly undeveloped at this time with some portions containing existing roadways of various types and phases of construction associated with adjacent developments that have occurred over time. The project area begins at Black Forest Road, which is the eastern boundary of the Wolf Ranch subdivision currently under development and is also currently the eastern boundary of the City of Colorado Springs. The terminus of the project area is along the Stapleton Road right-of-way at Meridian Road.

There is currently a large amount of development in this rapidly developing area of the City and the County. While the majority of the project area is currently within the jurisdiction of the County, it is likely that a significant portion of the currently developing areas within the corridor may be incorporated into the City as development progresses.

One of the primary purposes of this project is to verify and develop the master plan requirements for the roadway. The County's 2040 Major Transportation Corridors Plan (MTCP) included specific recommendations regarding functional

classification, transportation modes, and other uses for the Briargate-Stapleton corridor. The MTCP indicates that corridor is expected to be a four-lane principal arterial from the eastern City Limits of Colorado Springs (Black Forest Road) to Judge Orr Road. It is anticipated that this project will plan for the ultimate improvements, but that interim phases of capacity and safety improvements may be warranted based upon the findings of the studies and funding limitations. The corridor will also be evaluated to determine if additional mobility provisions such as bike route, pedestrian accommodations, and public transit are necessary.

The preferred alternative will reflect corridor improvements that optimize public safety, needs, and preferences while balancing enhanced capacity, access management, and development.”

You may also access the many plans and reports associated with the project by clicking on their links (your cursor will change from arrow to a hand when you hover over a link).

Your input, which has been recorded for review, plays a valuable role in shaping the conceptual design of the roadway, designed to improve access and enhance safety for the entire community and provide long-term benefits for El Paso County. As requested, you have been added to the project mailing list.

We appreciate your continued participation!

Maureen Paz de Araujo, FAICP CTP CEP  
Senior Transportation Planner

Tracked: Y



**File/Record Number: 12**

**From:** Anthony Hicks <reply-to+9b0ca7e3fdb0@crm.wix.com>  
**Sent:** Saturday, August 13, 2022, 2:09 PM  
**To:** Paz de Araujo, Maureen <Maureen.PazdeAraujo@wilsonco.com>;  
 briargatestapleton@gmail.com  
**Subject:** [briargate-stapleton] Contacts Form - new submission

**Anthony Hicks** just submitted your form: Contacts Form  
 on [briargate-stapleton](#)

**Message Details:**

Name: Anthony Hicks Email: [tony.hicks@att.net](mailto:tony.hicks@att.net)

Subject: Impact of this development on Highland Park Subdivision

Message: Please keep me posted on your development since it seems to have some major impact on our subdivision. Many thanks.

Add me to the project mailing list.: Checked

**Response 06/19/23**

Dear Anthony Hicks:

Thank you for connecting with us through our Briargate-Stapleton Project for Mobility website, by using our interactive map, and/or via email to share your roadway safety concerns and improvement preferences, including your concerns regarding the project's impact on the Highland Park subdivision. As requested, you have been added to the project mailing list.

Since the 30-day public comment period closed in September 2022, the project encountered some delay, but is now ready to progress in earnest. It is anticipated that both the Corridor Preservation Plan and the Access Control Plan will be presented to the Planning Commission and the Board of County Commissioners in mid to late summer for review and adoption, respectively. Presentation to the Planning Commission will occur at two regular meetings, the first as a public hearing. The County will advertise the specific dates of these meetings and meeting notices will also be posted on the project website.

Stay up to date by accessing our comprehensive website: <https://www.briargate-stapleton.com/>.

We appreciate your continued participation!

Maureen Paz de Araujo, FAICP CTP CEP  
 Senior Transportation Planner

**Tracked: Y**



**File/Record Number: 13**

**From:** Sally Miller <reply-to+8c7c13c08d92@crm.wix.com>  
**Sent:** Saturday, August 13, 2022, 5:30 PM  
**To:** Paz de Araujo, Maureen <Maureen.PazdeAraujo@wilsonco.com>;  
 briargatestapleton@gmail.com  
**Subject:** [briargate-stapleton] Contacts Form - new submission

**Sally Miller** just submitted your form: Contacts Form  
 on [briargate-stapleton](#)

**Message Details:**

Name: Sally Miller

Email: [sallysuemiller@gmail.com](mailto:sallysuemiller@gmail.com)

Subject: BRIARGATE STAPLETON

Message: -

Add me to the project mailing list.: Checked

**Response 06/19/23**

Dear Sally Miller:

Thank you for connecting with us through our Briargate-Stapleton Project for Mobility website, by using our interactive map, and/or via email. As requested, you have been added to the project mailing list.

Since the 30-day public comment period closed in September 2022, the project encountered some delay, but is now ready to progress in earnest. It is anticipated that both the Corridor Preservation Plan and the Access Control Plan will be presented to the Planning Commission and the Board of County Commissioners in mid to late summer for review and adoption, respectively. Presentation to the Planning Commission will occur at two regular meetings, the first as a public hearing. The County will advertise the specific dates of these meetings and meeting notices will also be posted on the project website.

Stay up to date by accessing our comprehensive website: <https://www.briargate-stapleton.com/>.

We appreciate your continued participation!

Maureen Paz de Araujo, FAICP CTP CEP  
 Senior Transportation Planner

**Tracked: Y**

**File/Record Number: 14a**

**From:** Debbie Christian <reply-to+11f2569d96b5@crm.wix.com>  
**Sent:** Saturday, August 13, 2022, 11:12 PM  
**To:** Paz de Araujo, Maureen <Maureen.PazdeAraujo@wilsonco.com>;  
briargatestapleton@gmail.com  
**Subject:** [briargate-stapleton] Contacts Form - new submission

**Debbie Christian** just submitted your form: Contacts Form  
on [briargate-stapleton](#)

**Message Details:**

Name: Debbie Christian

Email: [debchristian7@gmail.com](mailto:debchristian7@gmail.com)

Subject: -

Message: -Add me to the project mailing list.: Checked

**Response 06/19/23**

Dear Debbie Christian:

Thank you for connecting with us through our Briargate-Stapleton Project for Mobility website, by using our interactive map, and/or via email. As requested, you have been added to the project mailing list.

Since the 30-day public comment period closed in September 2022, the project encountered some delay, but is now ready to progress in earnest. It is anticipated that both the Corridor Preservation Plan and the Access Control Plan will be presented to the Planning Commission and the Board of County Commissioners in mid to late summer for review and adoption, respectively. Presentation to the Planning Commission will occur at two regular meetings, the first as a public hearing. The County will advertise the specific dates of these meetings and meeting notices will also be posted on the project website.

Stay up to date by accessing our comprehensive website: <https://www.briargate-stapleton.com/>.

We appreciate your continued participation!

Maureen Paz de Araujo, FAICP CTP CEP  
Senior Transportation Planner

**Tracked: Y**

**File/Record Number: 14a**

**From:** Debbie Ardan Christian <reply-to+d9de8c206771@crm.wix.com>  
**Sent:** Sunday, August 14, 2022, 11:28 PM  
**To:** Paz de Araujo, Maureen <Maureen.PazdeAraujo@wilsonco.com>;  
briargatestapleton@gmail.com  
**Subject:** [briargate-stapleton] Contacts Form - new submission

**Debbie Ardan Christian** just submitted your form: Contacts Form  
on [briargate-stapleton](#)

**Message Details:**

Name: Debbie Ardan Christian Email: [debchristian7@gmail.com](mailto:debchristian7@gmail.com)

Subject: -

Message: -Add me to the project mailing list.: Checked

**Response 00/00/22 N/A**

**DUPLICATE EMAIL: See File/Record No. 14a.**

Dear Debbie Arden Christian:

Thank you for connecting with us through our Briargate-Stapleton Project for Mobility website, by using our interactive map, and/or via email. As requested, you have been added to the project mailing list.

Stay up to date by accessing our comprehensive website: <https://www.briargate-stapleton.com/>.

We appreciate your continued participation!

Maureen Paz de Araujo, FAICP CTP CEP  
Senior Transportation Planner

**Tracked: Y**

**File/Record Number: 15**

**From:** Lori Yoder <reply-to+4ffd05005631@crm.wix.com>  
**Sent:** Sunday, August 14, 2022, 12:05 AM  
**To:** Paz de Araujo, Maureen <Maureen.PazdeAraujo@wilsonco.com>;  
 briargatestapleton@gmail.com  
**Subject:** [briargate-stapleton] Contacts Form - new submission

Lori Yoder just submitted your form: Contacts Form  
 on [briargate-stapleton](#)

**Message Details:**

Name: Lori Yoder

Email: [crafty1462@aim.com](mailto:crafty1462@aim.com)

Subject: Briargate - Stapleton project

Message: I HIGHLY disapprove of having commercial buildings along the proposed corridor off Vollmer Rd! We live out here to be AWAY from the city and it's a crime. I realize a ROAD is needed to alleviate the traffic congestion with all the overbuilding in the area. The infrastructure can't support all the homes being put in. But adding to those commercial buildings is NOT the answer!! Make it a park, or hiking trails, but DO NOT put in commercial businesses!

Add me to the project mailing list.: Checked

**Response 06/19/23**

Dear Lori Yoder:

Thank you for connecting with El Paso County through our Briargate-Stapleton Project for Mobility website, by using our interactive map, and/or via email to share your roadway safety concerns and improvement preferences, including your strong disapproval of having commercial buildings along the proposed corridor off Vollmer Road. Your input, which has been recorded for review, plays a valuable role in shaping the conceptual design of the roadway, designed to improve access and enhance safety for the entire community and provide long-term benefits for El Paso County.

Zoning is outside the scope of this study. The current zoning in the area stems from actions in 2016 to 2021. These plats do not have any new updates along Vollmer Road area. The current uses and development are within the adopted zoning regulation for El Paso County in the project area. There are several zoning classifications in the project area that include residential and commercial uses. The conceptual design and access control for the roadway corridor is consistent with currently adopted zoning. Please contact El Paso County Planner of the Day at 719-520-6944 for more information about study area zoning and permitted land uses.

As requested, you have been added to the project mailing list.

Since the 30-day public comment period closed in September 2022, the project encountered some delay, but is now ready to progress in earnest. It is anticipated that both the Corridor Preservation Plan and the Access Control Plan will be presented to the Planning Commission and the Board of County Commissioners in mid to late summer for review and adoption, respectively. Presentation to the Planning Commission will occur at two regular meetings, the first as a public hearing. The County will advertise the specific dates of these meetings and meeting notices will also be posted on the project website.

Stay up to date by accessing our comprehensive website: <https://www.briargate-stapleton.com/>.

We appreciate your continued participation!

Maureen Paz de Araujo, FAICP CTP CEP  
 Senior Transportation Planner

**Tracked: Y**

**File/Record Number: 16**

**From:** Marcia Hunt <reply-to+fa0a034ad5a3@crm.wix.com>  
**Sent:** Sunday, August 14, 2022, 6:27 AM  
**To:** Paz de Araujo, Maureen <Maureen.PazdeAraujo@wilsonco.com>;  
 briargatestapleton@gmail.com  
**Subject:** [briargate-stapleton] Contacts Form - new submission

**Marcia Hunt** just submitted your form: Contacts Form  
 on [briargate-stapleton](#)

**Message Details:**

Name: Marcia Hunt

Email: [marciahunt25@gmail.com](mailto:marciahunt25@gmail.com)

Subject: Width of road

Message: Hello, I'd like to propose that you consider the width of this road be 3 lanes in each direction (or a minimum of 4). This will be only the 2nd road (next to Woodmen) that travels east/west from I25 to Highway 24! Look at the amount of congestion we see on woodmen road, and how many of us currently use it. Expect the same amount, or more on Briargate-Stapleton. Please, please, don't do this like the extension of Dublin where it's only 2 lanes...or heaven forbid, Marksheffel. Clearly this community is too big NOT to plan for additional growth and usage. Thank you!

Add me to the project mailing list.: Checked

**Response 00/00/22**

Dear Marcia Hunt:

Thank you for connecting with El Paso County through our Briargate-Stapleton Project for Mobility website, by using our interactive map, and/or via email to share your roadway safety concerns and improvement preferences, including your suggestion to widen the roadway to include additional lanes to accommodate future growth. Your input, which has been recorded for review, plays a valuable role in shaping the conceptual design of the roadway, designed to improve access and enhance safety for the entire community and provide long-term benefits for El Paso County.

As part of the compliance with the El Paso County standards, a four-lane facility is planned with curb and gutter, two 11-foot travel lanes in each direction, shoulders, a sidewalk of multipurpose trail on each side of the roadway, and a utilities corridor/easement located on one or both side of the roadway corridor. Right-of-way and/or utility easements to accommodate this type of improvement was taken into consideration and included in the platting for the subdivision's footprint. The final alignment will depend on the public input from this study to determine the access management.

As requested, you have been added to the project mailing list.

Since the 30-day public comment period closed in September 2022, the project encountered some delay, but is now ready to progress in earnest. It is anticipated that both the Corridor Preservation Plan and the Access Control Plan will be presented to the Planning Commission and the Board of County Commissioners in mid to late summer for review and adoption, respectively. Presentation to the Planning Commission will occur at two regular meetings, the first as a public hearing. The County will advertise the specific dates of these meetings and meeting notices will also be posted on the project website.

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We appreciate your continued participation!

Maureen Paz de Araujo, FAICP CTP CEP  
 Senior Transportation Planner

**Tracked: Y**

**From:** [Paz de Araujo, Maureen](#)  
**To:** [Schlosberg, Wendy](#)  
**Subject:** FW: [briargate-stapleton] Contacts Form - new submission  
**Date:** Tuesday, September 20, 2022 10:59:18 AM

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**File/Record Number: 17**

**From:** Charles White <reply-to+ad0a8b686ac9@crm.wix.com>  
**Sent:** Sunday, August 14, 2022, 6:30 AM  
**To:** Paz de Araujo, Maureen <Maureen.PazdeAraujo@wilsonco.com>;  
briargatestapleton@gmail.com  
**Subject:** [briargate-stapleton] Contacts Form - new submission

**Charles White** just submitted your form: Contacts Form  
on [briargate-stapleton](#)

**Message Details:**

Name: Charles White

Email: [cwhite0703@yahoo.com](mailto:cwhite0703@yahoo.com)

Subject: -

Message: -

Add me to the project mailing list.: Checked

**Response 06/19/23**

Dear Charles White:

Thank you for connecting with us through our Briargate-Stapleton Project for Mobility website, by using our interactive map, and/or via email. As requested, you have been added to the project mailing list.

Since the 30-day public comment period closed in September 2022, the project encountered some delay, but is now ready to progress in earnest. It is anticipated that both the Corridor Preservation Plan and the Access Control Plan will be presented to the Planning Commission and the Board of County Commissioners in mid to late summer for review and adoption, respectively. Presentation to the Planning Commission will occur at two regular meetings, the first as a public hearing. The County will advertise the specific dates of these meetings and meeting notices will also be posted on the project website.

Stay up to date by accessing our comprehensive website: <https://www.briargate-stapleton.com/>.

We appreciate your continued participation!

Maureen Paz de Araujo, FAICP CTP CEP  
Senior Transportation Planner

**Tracked: Y**

**File/Record Number: 18**

**From:** Theodore Braun <reply-to+cd04bd515aa5@crm.wix.com>  
**Sent:** Sunday, August 14, 2022, 6:34 AM  
**To:** Paz de Araujo, Maureen <Maureen.PazdeAraujo@wilsonco.com>;  
briargatestapleton@gmail.com  
**Subject:** [briargate-stapleton] Contacts Form - new submission

Theodore Braun just submitted your form: Contacts Form  
on [briargate-stapleton](#)

**Message Details:**

Name: Theodore Braun

Email: [bluemustang26@hotmail.com](mailto:bluemustang26@hotmail.com)

Subject: Stapleton project

Message: -

Add me to the project mailing list.: Checked

**Response 06/19/23**

Dear Theodore Braun:

Thank you for connecting with us through our Briargate-Stapleton Project for Mobility website, by using our interactive map, and/or via email. As requested, you have been added to the project mailing list.

Since the 30-day public comment period closed in September 2022, the project encountered some delay, but is now ready to progress in earnest. It is anticipated that both the Corridor Preservation Plan and the Access Control Plan will be presented to the Planning Commission and the Board of County Commissioners in mid to late summer for review and adoption, respectively. Presentation to the Planning Commission will occur at two regular meetings, the first as a public hearing. The County will advertise the specific dates of these meetings and meeting notices will also be posted on the project website.

Stay up to date by accessing our comprehensive website: <https://www.briargate-stapleton.com/>.

We appreciate your continued participation!

Maureen Paz de Araujo, FAICP CTP CEP  
Senior Transportation Planner

**Tracked: Y**

**File/Record Number: 19**

**From:** Susan Scheppele <reply to:f4ab791b269c@crm.wix.com>  
**Sent:** Sunday, August 14, 2022, 7:11 AM  
**To:** Paz de Araujo, Maureen <Maureen.PazdeAraujo@wilsonco.com>;  
briargatestapleton@gmail.com  
**Subject:** [briargate-stapleton] Contacts Form - new submission

**Susan Scheppele** just submitted your form: Contacts Form  
on [briargate-stapleton](#)

**Message Details:**

Name: Susan Scheppele

Email: [outdoorlovers501@gmail.com](mailto:outdoorlovers501@gmail.com)

Subject: Briargate-Stapleton Message: Falcon Resident

Add me to the project mailing list.: Checked

Response 00/00/22

Dear Susan Scheppele:

Thank you for connecting with us through our Briargate-Stapleton Project for Mobility website, by using our interactive map, and/or via email. As requested, you have been added to the project mailing list.

Since the 30-day public comment period closed in September 2022, the project encountered some delay, but is now ready to progress in earnest. It is anticipated that both the Corridor Preservation Plan and the Access Control Plan will be presented to the Planning Commission and the Board of County Commissioners in mid to late summer for review and adoption, respectively. Presentation to the Planning Commission will occur at two regular meetings, the first as a public hearing. The County will advertise the specific dates of these meetings and meeting notices will also be posted on the project website.

Stay up to date by accessing our comprehensive website: <https://www.briargate-stapleton.com/>.

We appreciate your continued participation!

Maureen Paz de Araujo, FAICP CTP CEP  
Senior Transportation Planner

Tracked: Y

**File/Record Number: 20**

**From:** Terrill McCall <reply-to+06030ddfb646@crm.wix.com>  
**Sent:** Sunday, August 14, 2022, 7:32 AM  
**To:** Paz de Araujo, Maureen <Maureen.PazdeAraujo@wilsonco.com>;  
 briargatestapleton@gmail.com  
**Subject:** [briargate-stapleton] Contacts Form - new submission

**Terrill McCall** just submitted your form: Contacts Form  
 on [briargate-stapleton](#)

**Message Details:**

Name: Terrill McCall

Email: [Terrillmccall@hotmail.com](mailto:Terrillmccall@hotmail.com)

Subject: Commercial development constraint

Message: As in Germany, the US needs to take a lesson learned and work more to contain commercial growth into urban and more so rural areas.

Add me to the project mailing list.: Checked

**Response 00/00/22**

Dear Terrill McCall:

Thank you for connecting with us through our Briargate-Stapleton Project for Mobility website, by using our interactive map, and/or via email. As requested, you have been added to the project mailing list.

Since the 30-day public comment period closed in September 2022, the project encountered some delay, but is now ready to progress in earnest. It is anticipated that both the Corridor Preservation Plan and the Access Control Plan will be presented to the Planning Commission and the Board of County Commissioners in mid to late summer for review and adoption, respectively. Presentation to the Planning Commission will occur at two regular meetings, the first as a public hearing. The County will advertise the specific dates of these meetings and meeting notices will also be posted on the project website.

Stay up to date by accessing our comprehensive website: <https://www.briargate-stapleton.com/>.

We appreciate your continued participation!

Maureen Paz de Araujo, FAICP CTP CEP  
 Senior Transportation Planner

**Tracked: Y**

**File/Record Number: 21**

**From:** Kevan Lacey <reply-to+9b62081099fb@crm.wix.com>  
**Sent:** Sunday, August 14, 2022, 7:39 AM  
**To:** Paz de Araujo, Maureen <Maureen.PazdeAraujo@wilsonco.com>;  
briargatestapleton@gmail.com  
**Subject:** [briargate-stapleton] Contacts Form - new submission

**Kevan Lacey** just submitted your form: Contacts Form  
on [briargate-stapleton](#)

**Message Details:**

Name: Kevan Lacey

Email: [kevandl87@gmail.com](mailto:kevandl87@gmail.com)

Subject: -

Message: -

Add me to the project mailing list.: Checked

**Response 06/19/23**

Dear Kevan Lacey:

Thank you for connecting with us through our Briargate-Stapleton Project for Mobility website, by using our interactive map, and/or via email. As requested, you have been added to the project mailing list.

Since the 30-day public comment period closed in September 2022, the project encountered some delay, but is now ready to progress in earnest. It is anticipated that both the Corridor Preservation Plan and the Access Control Plan will be presented to the Planning Commission and the Board of County Commissioners in mid to late summer for review and adoption, respectively. Presentation to the Planning Commission will occur at two regular meetings, the first as a public hearing. The County will advertise the specific dates of these meetings and meeting notices will also be posted on the project website.

Stay up to date by accessing our comprehensive website: <https://www.briargate-stapleton.com/>.

We appreciate your continued participation!

Maureen Paz de Araujo, FAICP CTP CEP  
Senior Transportation Planner

**Tracked: Y**

**File/Record Number: 22a**

**See Record 22e for single response to 22a–22e.**

**From:** rrraulreyes@gmail.com <reply-to+87169ee41e63@crm.wix.com>

**Sent:** Sunday, August 14, 2022, 8:18 AM

**To:** Paz de Araujo, Maureen <Maureen.PazdeAraujo@wilsonco.com>;  
briargatestapleton@gmail.com

**Subject:** [briargate-stapleton] Contacts Form - new submission

**A site visitor** just submitted your form: Contacts Form  
on [briargate-stapleton](#)

### Message Details:

Name: -

Email: [rrraulreyes@gmail.com](mailto:rrraulreyes@gmail.com)

Subject: -

Message: -

Add me to the project mailing list.: Checked

Response 00/00/22 **N/A See 22e for response to 22a–22e.**

**Tracked: Y**

**File/Record Number: 22e**

**See Record 22e for single response to 22a–22e.**

**From:** rrraulreyes@gmail.com <reply-to+4a6a39ed03ab@crm.wix.com>

**Sent:** Sunday, August 14, 2022, 8:18 AM

**To:** Paz de Araujo, Maureen <Maureen.PazdeAraujo@wilsonco.com>;  
briargatestapleton@gmail.com

**Subject:** [briargate-stapleton] Contacts Form - new submission

**A site visitor** just submitted your form: Contacts Form  
on [briargate-stapleton](#)

### Message Details:

Name: -

Subject: -

Message: -

Add me to the project mailing list.: Checked

Response 00/00/22 **N/A See 22e for response to 22a–22e.**

**Tracked: Y**



**File/Record Number: 22e**

**See Record 22e for single response to 22a-22e.**

**From:** Raul Reyes <reply-to+a64f28809b20@crm.wix.com>

**Sent:** Sunday, August 14, 2022, 8:18 AM

**To:** Paz de Araujo, Maureen <Maureen.PazdeAraujo@wilsonco.com>;  
briargatestapleton@gmail.com

**Subject:** [briargate-stapleton] Contacts Form - new submission

**Raul Reyes** just submitted your form: Contacts Form  
on [briargate-stapleton](#)

**Message Details:**

Name: Raul Reyes

Email: [rrraulreyes@gmail.com](mailto:rrraulreyes@gmail.com)

Subject: -

Message: -

Add me to the project mailing list.: Unchecked

Response 00/00/22 **N/A See 22e for response to 22a-22e.**

**Tracked: Y**

**File/Record Number: 22d**

**See Record 22e for single response to 22a–22e.**

**From:** Raul Reyes <reply-to+c2e16781ca8f@crm.wix.com>

**Sent:** Sunday, August 14, 2022, 8:19 AM

**To:** Paz de Araujo, Maureen <Maureen.PazdeAraujo@wilsonco.com>;  
briargatestapleton@gmail.com

**Subject:** [briargate-stapleton] Contacts Form - new submission

**Raul Reyes** just submitted your form: Contacts Form  
on [briargate-stapleton](#)

**Message Details:**

Name: Raul Reyes

Email: [rrraulreyes@gmail.com](mailto:rrraulreyes@gmail.com)

Subject: Rezoning

Message: -

Add me to the project mailing list.: Checked

Response 00/00/22 **N/A See 22e for response to 22a–22e.**

**Tracked: Y**



**File/Record Number: 22e Record 22e is single response to 22a-22e.**

**From:** Raul Reyes <reply-to+818d2f02faf8@crm.wix.com>  
**Sent:** Sunday, August 14, 2022, 8.30 AM  
**To:** Paz de Araujo, Maureen <Maureen.PazdeAraujo@wilsonco.com>;  
 briargatestapleton@gmail.com  
**Subject:** [briargate-stapleton] Contacts Form - new submission

**Raul Reyes** just submitted your form: Contacts Form  
 on [briargate-stapleton](#)

### Message Details:

Name: Raul Reyes

Email: [rrraulreyes@gmail.com](mailto:rrraulreyes@gmail.com)

Subject: Object

Message: My family and I STRONGLY object to rezoning this parcel into Commercial Development. This is and has been a historically family zoned area with many of us owning horses that we ride in the area and with many children that ride their bikes and walk in the area. Zoning this area commercial will create a high traffic area that will be especially dangerous to the many children and adults that live and recreate here. Please consider making this a 2 1/2 are home development site, or better still, make it a beautiful community park for the many families that are moving into the area due to the many homes being built on Powers.

Add me to the project mailing list.: Checked

### Response 06/19/23

Dear Raul Reyes:

Thank you for connecting with El Paso County through our Briargate-Stapleton Project for Mobility website, by using our interactive map, and/or via email to share your roadway safety concerns and improvement preferences, including your strong objection to rezoning for commercial development.

Zoning is outside the scope of this study. The current zoning in the area stems from actions taken from 2016 through 2021. There have been no new updates to permitted land uses within the Vollmer Road area since that time. The current uses and development are within the adopted zoning regulation for El Paso County in the project area. There are several zoning classifications in the project area that include residential and commercial uses. The conceptual design and access control for the roadway corridor is consistent with currently adopted zoning. Please contact El Paso County Planner of the Day at 719-520-6944 for more information about study area zoning and permitted land uses. The conceptual design follows the currently adopted zoning regulations. Please contact El Paso County with specific questions at the *Planner of the Day* at 719-520-6944 for more information about study area zoning and permitted land uses.

Your input, which has been recorded for review, plays a valuable role in shaping the conceptual design of the roadway, designed to improve access and enhance safety for the entire community and provide long-term benefits for El Paso County.

As requested, you have been added to the project mailing list.

Since the 30-day public comment period closed in September 2022, the project encountered some delay, but is now ready to progress in earnest. It is anticipated that both the Corridor Preservation Plan and the Access Control Plan will be presented to the Planning Commission and the Board of County Commissioners in mid to late summer for review and adoption, respectively. Presentation to the Planning Commission will occur at two regular meetings, the first as a public hearing. The County will advertise the specific dates of these meetings and meeting notices will also be posted on the project website.

Stay up to date by accessing our comprehensive website: <https://www.briargate-stapleton.com/>.

We appreciate your continued participation!

Maureen Paz de Araujo, FAICP CTP CEP  
Senior Transportation Planner

Tracked: Y

**File/Record Number: 23**

**From:** Rae <reply-to+201a3c618dff@crm.wix.com>  
**Sent:** Sunday, August 14, 2022, 8:32 AM  
**To:** Paz de Araujo, Maureen <Maureen.PazdeAraujo@wilsonco.com>;  
 briargatestapleton@gmail.com  
**Subject:** [briargate-stapleton] Contacts Form - new submission

Rae just submitted your form: Contacts Form  
 on [briargate-stapleton](#)

**Message Details:**

Name: Rae

Email: [raerae84@gmail.com](mailto:raerae84@gmail.com)

Subject: None

Message: None

Add me to the project mailing list.: Unchecked

**Response 06/19/23**

Dear Rae:

Thank you for connecting with us through our Briargate-Stapleton Project for Mobility website, by using our interactive map, and/or via email. As requested, you have been removed from the project mailing list.

Since the 30-day public comment period closed in September 2022, the project encountered some delay, but is now ready to progress in earnest. It is anticipated that both the Corridor Preservation Plan and the Access Control Plan will be presented to the Planning Commission and the Board of County Commissioners in mid to late summer for review and adoption, respectively. Presentation to the Planning Commission will occur at two regular meetings, the first as a public hearing. The County will advertise the specific dates of these meetings and meeting notices will also be posted on the project website.

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We appreciate your continued participation!

Maureen Paz de Araujo, FAICP CTP CEP  
 Senior Transportation Planner

**Tracked: Y**

**File/Record Number: 24**

**From:** Kelle LeRoy <reply-to+286ae288a270@crm.wix.com>  
**Sent:** Sunday, August 14, 2022, 8:41 AM  
**To:** Paz de Araujo, Maureen <Maureen.PazdeAraujo@wilsonco.com>;  
 briargatestapleton@gmail.com  
**Subject:** [briargate-stapleton] Contacts Form - new submission

Kelle LeRoy just submitted your form: Contacts Form  
 on [briargate-stapleton](#)

**Message Details:**

Name: Kelle LeRoy  
 Email: [leroytk@comcast.net](mailto:leroytk@comcast.net)  
 Subject: Briargate Parkway-Stapleton Project  
 Message: Interested in receiving information to review project plan. Thank you  
 Add me to the project mailing list.: Checked

**Response 06/19/23**

Dear Kelle LeRoy:

Thank you for connecting with El Paso County through our Briargate-Stapleton Project for Mobility website, by using our interactive map, and/or via email to share your roadway safety concerns and improvement preferences, including your interest in reviewing the project plan. Your input, which has been recorded for review, plays a valuable role in shaping the conceptual design of the roadway, designed to improve access and enhance safety for the entire community and provide long-term benefits for El Paso County.

To review project plans and other project-related documents, please access our comprehensive website at:  
<https://www.briargate-stapleton.com/>.

At the very top of the homepage, please click on the "Project Resources" link. This will bring you to project documents grouped into the following categories:

1. **Related Planning Studies:** Click "Learn More" to access the studies.
2. **Briargate Stapleton Project Documents:** Click "Learn More" to access the project documents.
3. **Study Area Development Plans:** Click "Learn More" to access the development plans.

As requested, you have been added to the project mailing list.

Since the 30-day public comment period closed in September 2022, the project encountered some delay, but is now ready to progress in earnest. It is anticipated that both the Corridor Preservation Plan and the Access Control Plan will be presented to the Planning Commission and the Board of County Commissioners in mid to late summer for review and adoption, respectively. Presentation to the Planning Commission will occur at two regular meetings, the first as a public hearing. The County will advertise the specific dates of these meetings and meeting notices will also be posted on the project website.

Stay up to date by accessing our comprehensive website: <https://www.briargate-stapleton.com/>.

We appreciate your continued participation!

Maureen Paz de Araujo, FAICP CTP CEP  
 Senior Transportation Planner

**Tracked: Y**





**File/Record Number: 25**

**From:** Jessica Riley <reply-to+0fe120c41793@crm.wix.com>  
**Sent:** Sunday, August 14, 2022, 8:43 AM  
**To:** Paz de Araujo, Maureen <Maureen.PazdeAraujo@wilsonco.com>;  
briargatestapleton@gmail.com  
**Subject:** [briargate-stapleton] Contacts Form - new submission

**Jessica Riley** just submitted your form: Contacts Form  
on [briargate-stapleton](#)

**Message Details:**

Name: Jessica Riley

Email: [jess86riley@gmail.com](mailto:jess86riley@gmail.com)

Subject: -

Message: -

Add me to the project mailing list.: Unchecked

**Response 06/19/23**

Dear Jessica Riley:

Thank you for connecting with us through our Briargate-Stapleton Project for Mobility website, by using our interactive map, and/or via email. As requested, you have been added to the project mailing list.

Since the 30-day public comment period closed in September 2022, the project encountered some delay, but is now ready to progress in earnest. It is anticipated that both the Corridor Preservation Plan and the Access Control Plan will be presented to the Planning Commission and the Board of County Commissioners in mid to late summer for review and adoption, respectively. Presentation to the Planning Commission will occur at two regular meetings, the first as a public hearing. The County will advertise the specific dates of these meetings and meeting notices will also be posted on the project website.

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We appreciate your continued participation!

Maureen Paz de Araujo, FAICP CTP CEP  
Senior Transportation Planner

**Tracked: Y**

**File/Record Number: 26**

**From:** Jessica Lawyer <reply-to+afbdcdfd80b@crm.wix.com>  
**Sent:** Sunday, August 14, 2022, 9:10 AM  
**To:** Paz de Araujo, Maureen <Maureen.PazdeAraujo@wilsonco.com>;  
briargatestapleton@gmail.com  
**Subject:** [briargate-stapleton] Contacts Form - new submission

**Jessica Lawyer** just submitted your form: Contacts Form  
on [briargate-stapleton](#)

**Message Details:**

Name: Jessica Lawyer

Email: [ritzil@hotmail.com](mailto:ritzil@hotmail.com)

Subject: -

Message: -

Add me to the project mailing list.: Checked

**Response 06/19/23**

Dear Jessica Lawyer:

Thank you for connecting with us through our Briargate-Stapleton Project for Mobility website, by using our interactive map, and/or via email. As requested, you have been added to the project mailing list.

Since the 30-day public comment period closed in September 2022, the project encountered some delay, but is now ready to progress in earnest. It is anticipated that both the Corridor Preservation Plan and the Access Control Plan will be presented to the Planning Commission and the Board of County Commissioners in mid to late summer for review and adoption, respectively. Presentation to the Planning Commission will occur at two regular meetings, the first as a public hearing. The County will advertise the specific dates of these meetings and meeting notices will also be posted on the project website.

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We appreciate your continued participation!

Maureen Paz de Araujo, FAICP CTP CEP  
Senior Transportation Planner

**Tracked: Y**

**File/Record Number: 27**

**From:** Caleb <reply-to+baa9c60dd13a@crm.wix.com>  
**Sent:** Sunday, August 14, 2022, 12:43 PM  
**To:** Paz de Araujo, Maureen <Maureen.PazdeAraujo@wilsonco.com>;  
 briargatestapleton@gmail.com  
**Subject:** [briargate-stapleton] Contacts Form - new submission

Caleb just submitted your form: Contacts Form  
 on [briargate-stapleton](#)

**Message Details:**

Name: Caleb

Email: [calebvanwinkle@gmail.com](mailto:calebvanwinkle@gmail.com)

Subject: -

Message: -

Add me to the project mailing list.: Checked

**Response 06/19/23**

Dear Caleb Van Winkle:

Thank you for connecting with us through our Briargate-Stapleton Project for Mobility website, by using our interactive map, and/or via email. As requested, you have been added to the project mailing list.

Since the 30-day public comment period closed in September 2022, the project encountered some delay, but is now ready to progress in earnest. It is anticipated that both the Corridor Preservation Plan and the Access Control Plan will be presented to the Planning Commission and the Board of County Commissioners in mid to late summer for review and adoption, respectively. Presentation to the Planning Commission will occur at two regular meetings, the first as a public hearing. The County will advertise the specific dates of these meetings and meeting notices will also be posted on the project website.

Stay up to date by accessing our comprehensive website: <https://www.briargate-stapleton.com/>.

We appreciate your continued participation!

Maureen Paz de Araujo, FAICP CTP CEP  
 Senior Transportation Planner

**Tracked: Y**



**File/Record Number: 28**

**From:** Ryan Unseth <reply-to+5f37e492cfc8@crm.wix.com>  
**Sent:** Sunday, August 14, 2022, 4:41 PM  
**To:** Paz de Araujo, Maureen <Maureen.PazdeAraujo@wilsonco.com>;  
 briargatestapleton@gmail.com  
**Subject:** [briargate-stapleton] Contacts Form - new submission

**Ryan Unseth** just submitted your form: Contacts Form  
 on [briargate-stapleton](#)

**Message Details:**

Name: Ryan Unseth

Email: [ryanunseth@gmail.com](mailto:ryanunseth@gmail.com)

Subject: Info

Message: I live in the Elkhorn Estates neighborhood and would like info on the proposed roadway. Thank you.

Add me to the project mailing list.: Checked

The construction buildout of the roadway is not funded currently. The public input of this corridor management plan will determine the final amenities of the corridor, the roadway widths, and the tie into the regional trail near Sterling Ranch. Construction of the roadway will need to go through the Local and Federal steps to be funded in the future.

**Response 06/19/23**

Dear Ryan Unseth:

Thank you for connecting with El Paso County through our Briargate-Stapleton Project for Mobility website, by using our interactive map, and/or via email to share your roadway safety concerns and improvement preferences, your request for more information about the proposed roadway. Your input, which has been recorded for review, plays a valuable role in shaping the conceptual design of the roadway, designed to improve access and enhance safety for the entire community and provide long-term benefits for El Paso County.

Please access our comprehensive website at: <https://www.briargate-stapleton.com/>. There you will find links to all project and project-related documents in the following categories:

- **Related Planning Studies:** Click "Learn More" to access the studies.
- **Briargate Stapleton Project Documents:** Click "Learn More" to access the project documents.
- **Study Area Development Plans:** Click "Learn More" to access the development plans.

Please note that the roadway design will not be finalized until all public and stakeholder input has been considered. <<OK?>

As requested, you have been added to the project mailing list.

Since the 30-day public comment period closed in September 2022, the project encountered some delay, but is now ready to progress in earnest. It is anticipated that both the Corridor Preservation Plan and the Access Control Plan will be presented to the Planning Commission and the Board of County Commissioners in mid to late summer for review and adoption, respectively. Presentation to the Planning Commission will occur at two regular meetings, the first as a public hearing. The County will advertise the specific dates of these meetings and meeting notices will also be posted on the project website.

Stay up to date by accessing our comprehensive website: <https://www.briargate-stapleton.com/>.

We appreciate your continued participation!

Maureen Paz de Araujo, FAICP CTP CEP  
Senior Transportation Planner

Tracked: Y

**File/Record Number: 29**

**From:** Rick Christian <reply-to+04d34afb910d@crm.wix.com>  
**Sent:** Sunday, August 14, 2022, 10:59 PM  
**To:** Paz de Araujo, Maureen <Maureen.PazdeAraujo@wilsonco.com>;  
 briargatestapleton@gmail.com  
**Subject:** [briargate-stapleton] Contacts Form - new submission

**Rick Christian** just submitted your form: Contacts Form  
 on [briargate-stapleton](#)

**Message Details:**

Name: Rick Christian

Email: [rick@ricklychristian.com](mailto:rick@ricklychristian.com)

Subject: Mailing list

Message: -

Add me to the project mailing list.: Checked

**Response 06/19/23**

Dear Rick Christian:

Thank you for connecting with us through our Briargate-Stapleton Project for Mobility website, by using our interactive map, and/or via email. As requested, you have been added to the project mailing list.

Since the 30-day public comment period closed in September 2022, the project encountered some delay, but is now ready to progress in earnest. It is anticipated that both the Corridor Preservation Plan and the Access Control Plan will be presented to the Planning Commission and the Board of County Commissioners in mid to late summer for review and adoption, respectively. Presentation to the Planning Commission will occur at two regular meetings, the first as a public hearing. The County will advertise the specific dates of these meetings and meeting notices will also be posted on the project website.

Stay up to date by accessing our comprehensive website: <https://www.briargate-stapleton.com/>.

We appreciate your continued participation!

Maureen Paz de Araujo, FAICP CTP CEP  
 Senior Transportation Planner

**Tracked: Y**

**File/Record Number: 30**

**From:** Anna <reply-to+934e7931f9a8@crm.wix.com>  
**Sent:** Sunday, August 14, 2022, 6:35 AM  
**To:** Paz de Araujo, Maureen <Maureen.PazdeAraujo@wilsonco.com>;  
briargatestapleton@gmail.com  
**Subject:** [briargate-stapleton] Contacts Form - new submission

**Anna just submitted your form: Contacts Form**  
on [briargate-stapleton](#)

**Message Details:**

Name: Anna

Email: [corunner82@gmail.com](mailto:corunner82@gmail.com)

Subject: -

Message: -

Add me to the project mailing list.: Checked

**Response 06/19/23**

Dear Anna:

Thank you for connecting with us through our Briargate-Stapleton Project for Mobility website, by using our interactive map, and/or via email. As requested, you have been added to the project mailing list.

Since the 30-day public comment period closed in September 2022, the project encountered some delay, but is now ready to progress in earnest. It is anticipated that both the Corridor Preservation Plan and the Access Control Plan will be presented to the Planning Commission and the Board of County Commissioners in mid to late summer for review and adoption, respectively. Presentation to the Planning Commission will occur at two regular meetings, the first as a public hearing. The County will advertise the specific dates of these meetings and meeting notices will also be posted on the project website.

Stay up to date by accessing our comprehensive website: <https://www.briargate-stapleton.com/>.

We appreciate your continued participation!

Maureen Paz de Araujo, FAICP CTP CEP  
Senior Transportation Planner

**Tracked: Y**



**File/Record Number: 31**

**From:** Chris Bailey <reply-to+45369efa7c70@crm.wix.com>  
**Sent:** Monday, August 15, 2022, 11:43 AM  
**To:** Paz de Araujo, Maureen <Maureen.PazdeAraujo@wilsonco.com>;  
briargatestapleton@gmail.com  
**Subject:** [briargate-stapleton] Contacts Form - new submission

**Chris Bailey** just submitted your form: Contacts Form  
on [briargate-stapleton](#)

**Message Details:**

Name: Chris Bailey

Email: [cbaileyf16@yahoo.com](mailto:cbaileyf16@yahoo.com)

Subject: None

Message: None

Add me to the project mailing list.: Unchecked

**Response 06/19/23**

Dear Chris Bailey:

Thank you for connecting with us through our Briargate-Stapleton Project for Mobility website, by using our interactive map, and/or via email. As requested, you have been removed from the project mailing list.

Since the 30-day public comment period closed in September 2022, the project encountered some delay, but is now ready to progress in earnest. It is anticipated that both the Corridor Preservation Plan and the Access Control Plan will be presented to the Planning Commission and the Board of County Commissioners in mid to late summer for review and adoption, respectively. Presentation to the Planning Commission will occur at two regular meetings, the first as a public hearing. The County will advertise the specific dates of these meetings and meeting notices will also be posted on the project website.

Stay up to date by accessing our comprehensive website: <https://www.briargate-stapleton.com/>.

We appreciate your continued participation!

Maureen Paz de Araujo, FAICP CTP CEP  
Senior Transportation Planner

**Tracked: Y**

**File/Record Number: 32**

**From:** James Ferrier <reply-to+83820684a065@crm.wix.com>  
**Sent:** Monday, August 15, 2022, 2:49 PM  
**To:** Paz de Araujo, Maureen <Maureen.PazdeAraujo@wilsonco.com>;  
briargatestapleton@gmail.com  
**Subject:** [briargate-stapleton] Contacts Form - new submission

**James Ferrier** just submitted your form: Contacts Form  
on [briargate-stapleton](#)

### Message Details:

Name: James Ferrier

Email: [jafer1776@gmail.com](mailto:jafer1776@gmail.com)

Subject: Email updates

Message: Please keep me updated on this project. Add me to the project mailing list.: Checked

### Response 06/19/23

Dear James Ferrier:

Thank you for connecting with us through our Briargate-Stapleton Project for Mobility website, by using our interactive map, and/or via email. As requested, you have been added to the project mailing list.

Since the 30-day public comment period closed in September 2022, the project encountered some delay, but is now ready to progress in earnest. It is anticipated that both the Corridor Preservation Plan and the Access Control Plan will be presented to the Planning Commission and the Board of County Commissioners in mid to late summer for review and adoption, respectively. Presentation to the Planning Commission will occur at two regular meetings, the first as a public hearing. The County will advertise the specific dates of these meetings and meeting notices will also be posted on the project website.

Stay up to date by accessing our comprehensive website: <https://www.briargate-stapleton.com/>.

We appreciate your continued participation!

Maureen Paz de Araujo, FAICP CTP CEP  
Senior Transportation Planner

**Tracked: Y**



**File/Record Number: 33**

**From:** Jim Woudstra <reply-to+d70497d899eb@crm.wix.com>  
**Sent:** Tuesday, August 16, 2022, 6:07 PM  
**To:** Paz de Araujo, Maureen <Maureen.PazdeAraujo@wilsonco.com>;  
 briargatestapleton@gmail.com  
**Subject:** [briargate-stapleton] Contacts Form - new submission

**Jim Woudstra** just submitted your form: Contacts Form  
 on [briargate-stapleton](#)

**Message Details:**

Name: Jim Woudstra

Email: [jjowoudstra@gmail.com](mailto:jjowoudstra@gmail.com)

Subject: Opposed to proposed commercial development area west of Vollmer Road

Message: According to the El Paso County Zone Map 523 from the Development Services Department, the area south of Poco Rd and west of Vollmer Rd is zoned as RR-5. The entire area south, north and west of this triangular section has been developed into acreages ranging in size from 2.5 acres to over 10 acres. It would be very offensive to the current acreage owners in that area to turn this area into some sort of commercial development. The project plan states: "anticipated commercial development to the north and south of the future Briargate - Stapleton Road and includes a proposed commercial collector intersection. Just because this area has not yet been bought up by developers and turned into residential lots is not an appropriate justification to turn it into commercial property. Those of us living west of Vollmer Road on Poco Road have already had the quality of our rural area negatively affected by the development occurring on the east side of Vollmer Road in spite of our serious opposition. Now you are proposing that there be some type of commercial development right in the middle of our acreages. This would negatively affect our quality of life and certainly lower property values. I hope that you will seriously consider our concerns and keep this area zoned as RR-5. If you need commercial development, why not put it in the middle of Sterling Ranch!!!

Add me to the project mailing list.: Checked

**Response 06/19/23**

Dear Jim Woudstra:

Thank you for connecting with El Paso County through our Briargate-Stapleton Project for Mobility website, by using our interactive map, and/or via email to share your roadway safety concerns and improvement preferences, including your strong opposition to commercial development area west of Vollmer Road. Your input, which has been recorded for review, plays a valuable role in shaping the conceptual design of the roadway, designed to improve access and enhance safety for the entire community and provide long-term benefits for El Paso County.

Zoning is outside the scope of this study. The current zoning in the area stems from actions taken from 2016 through 2021. There have been no new updates to permitted land uses within the Vollmer Road area since that time. The current uses and development are within the adopted zoning regulation for El Paso County in the project area. There are several zoning classifications in the project area that include residential and commercial uses. The conceptual design and access control for the roadway corridor is consistent with currently adopted zoning. Please contact El Paso County Planner of the Day at 719-520-6944 for more information about study area zoning and permitted land uses. The conceptual design follows the currently adopted zoning regulations. Please contact El Paso County with specific questions at the *Planner of the Day* at 719-520-6944 for more information about study area zoning and permitted land uses.

As requested, you have been added to the project mailing list.

Since the 30-day public comment period closed in September 2022, the project encountered some delay, but is now ready to progress in earnest. It is anticipated that both the Corridor Preservation Plan and the Access Control Plan will be presented to the Planning Commission and the Board of County Commissioners in mid to late summer for review and adoption, respectively. Presentation to the Planning Commission will occur at two regular meetings, the first as a public hearing. The County will advertise the specific dates of these meetings and meeting notices will also be posted on the project website.

Stay up to date by accessing our comprehensive website: <https://www.briargate-stapleton.com/>.

We appreciate your continued participation!

Maureen Paz de Araujo, FAICP CTP CEP  
Senior Transportation Planner

Tracked: Y

**File/Record Number: 34**

**From:** rachonne@gmail.com <reply-to+37e1012948d9@crm.wix.com>

**Sent:** Tuesday, August 16, 2022, 8:49 PM

**To:** Paz de Araujo, Maureen <Maureen.PazdeAraujo@wilsonco.com>;  
briargatestapleton@gmail.com

**Subject:** [briargate-stapleton] Contacts Form - new submission

**A site visitor** just submitted your form: Contacts Form  
on [briargate-stapleton](#)

**Message Details:**

Name: -

Email: [rachonne@gmail.com](mailto:rachonne@gmail.com)

Subject: -

Message: -

Add me to the project mailing list.: Checked

**Response 06/19/23**

Dear Rachonne:

Thank you for connecting with us through our Briargate-Stapleton Project for Mobility website, by using our interactive map, and/or via email. As requested, you have been added to the project mailing list.

Since the 30-day public comment period closed in September 2022, the project encountered some delay, but is now ready to progress in earnest. It is anticipated that both the Corridor Preservation Plan and the Access Control Plan will be presented to the Planning Commission and the Board of County Commissioners in mid to late summer for review and adoption, respectively. Presentation to the Planning Commission will occur at two regular meetings, the first as a public hearing. The County will advertise the specific dates of these meetings and meeting notices will also be posted on the project website.

Stay up to date by accessing our comprehensive website: <https://www.briargate-stapleton.com/>.

We appreciate your continued participation!

Maureen Paz de Araujo, FAICP CTP CEP  
Senior Transportation Planner

**Tracked: Y**

**File/Record Number: 35**

**From:** Sara Schafer <reply-to+4e9ab125e1a5@crm.wix.com>  
**Sent:** Thursday, August 18, 2022, 7:08 AM  
**To:** Paz de Araujo, Maureen <Maureen.PazdeAraujo@wilsonco.com>;  
 briargatestapleton@gmail.com  
**Subject:** [briargate-stapleton] Contacts Form - new submission

**Sara Schafer** just submitted your form: Contacts Form  
 on [briargate-stapleton](#)

**Message Details:**

Name: Sara Schafer

Email: [mudrace1855@msn.com](mailto:mudrace1855@msn.com)

Subject: -

Message: -

Add me to the project mailing list.: Checked

Add me to the project mailing list.: Checked

**Response 06/19/23**

Dear Sara Schafer:

Thank you for connecting with us through our Briargate-Stapleton Project for Mobility website, by using our interactive map, and/or via email. As requested, you have been added to the project mailing list.

Since the 30-day public comment period closed in September 2022, the project encountered some delay, but is now ready to progress in earnest. It is anticipated that both the Corridor Preservation Plan and the Access Control Plan will be presented to the Planning Commission and the Board of County Commissioners in mid to late summer for review and adoption, respectively. Presentation to the Planning Commission will occur at two regular meetings, the first as a public hearing. The County will advertise the specific dates of these meetings and meeting notices will also be posted on the project website.

Stay up to date by accessing our comprehensive website: <https://www.briargate-stapleton.com/>.

We appreciate your continued participation!

Maureen Paz de Araujo, FAICP CTP CEP  
 Senior Transportation Planner

**Tracked: Y**

**File/Record Number: 36a**

**From:** Cheryl <reply-to+61476754f876@crm.wix.com>  
**Sent:** Thursday, August 18, 2022, 8:38 PM  
**To:** Paz de Araujo, Maureen <Maureen.PazdeAraujo@wilsonco.com>;  
briargatestapleton@gmail.com  
**Subject:** [briargate-stapleton] Contacts Form - new submission

**Cheryl** just submitted your form: Contacts Form  
on [briargate-stapleton](#)

**Message Details:**

Name: Cheryl

Email: [cheryl@graydons.com](mailto:cheryl@graydons.com)

Subject: -

Message: Pls add me

Add me to the project mailing list.: Checked

**Response 00/00/22 N/A: Response in Record 36b.**

Dear Cheryl:

Thank you for connecting with us through our Briargate-Stapleton Project for Mobility website, by using our interactive map, and/or via email. As requested, you have been added to the project mailing list.

Since the 30-day public comment period closed in September 2022, the project encountered some delay, but is now ready to progress in earnest. It is anticipated that both the Corridor Preservation Plan and the Access Control Plan will be presented to the Planning Commission and the Board of County Commissioners in mid to late summer for review and adoption, respectively. Presentation to the Planning Commission will occur at two regular meetings, the first as a public hearing. The County will advertise the specific dates of these meetings and meeting notices will also be posted on the [project website](#).

Stay up to date by accessing our comprehensive website: <https://www.briargate-stapleton.com/>.

We appreciate your continued participation!

Maureen Paz de Araujo, FAICP CTP CEP  
Senior Transportation Planner

**Tracked: Y**



**File/Record Number: 36b**

**From:** Cheryl <reply-to+a75aef0597ba@crm.wix.com>  
**Sent:** Friday, August 19, 2022, 8:22 AM  
**To:** Paz de Araujo, Maureen <Maureen.PazdeAraujo@wilsonco.com>;  
 briargatestapleton@gmail.com  
**Subject:** [briargate-stapleton] Contacts Form - new submission

Cheryl just submitted your form: Contacts Form  
 on [briargate-stapleton](#)

**Message Details:**

Name: Cheryl

Email: [cheryl@graydons.com](mailto:cheryl@graydons.com)

Subject: Briargate Stapleton corridor

Message: I didn't see a project timeline any where? Can you point me to it and add that to this info to the website? Thanks!

Add me to the project mailing list.: Checked

**Response 06/19/23**

Dear Cheryl:

Thank you for connecting with El Paso County through our Briargate-Stapleton Project for Mobility website, by using our interactive map, and/or via email to share your roadway safety concerns and improvement preferences, including your request for a project timeline. Your input, which has been recorded for review, plays a valuable role in shaping the conceptual design of the roadway, designed to improve access and enhance safety for the entire community and provide long-term benefits for El Paso County.

The construction buildout of the roadway is not funded at this time. The public input of this corridor management plan will determine the final amenities of the corridor, the roadway widths, and the tie into the regional trail near Sterling Ranch. Construction of the roadway will need to go through the Local and Federal requirements to be eligible for funding in the future.

As requested, you have been added to the project mailing list.

Since the 30-day public comment period closed in September 2022, the project encountered some delay, but is now ready to progress in earnest. It is anticipated that both the Corridor Preservation Plan and the Access Control Plan will be presented to the Planning Commission and the Board of County Commissioners in mid to late summer for review and adoption, respectively. Presentation to the Planning Commission will occur at two regular meetings, the first as a public hearing. The County will advertise the specific dates of these meetings and meeting notices will also be posted on the project website.

Stay up to date by accessing our comprehensive website: <https://www.briargate-stapleton.com/>.

We appreciate your continued participation!

Maureen Paz de Araujo, FAICP CTP CEP  
 Senior Transportation Planner

**Tracked: Y**

**File/Record Number: 37**

**From:** John G. Whitcomb <reply-to+c1ee6525da5e@crm.wix.com>  
**Sent:** Monday, August 22, 2022, 8:39 AM  
**To:** Paz de Araujo, Maureen <Maureen.PazdeAraujo@wilsonco.com>;  
 briargatestapleton@gmail.com  
**Subject:** [briargate-stapleton] Contacts Form - new submission

**John G. Whitcomb** just submitted your form: Contacts Form  
 on [briargate-stapleton](#)

**Message Details:**

Name: John G. Whitcomb

Email: [john.whitcomb@aiplaybooks.com](mailto:john.whitcomb@aiplaybooks.com)

Subject: Interested Community Member

Message: The Denver City Council cast Stapleton into the dustbin of history on Monday, rebranding the northeast neighborhood area as Central Park as a means of distancing it from a former mayor who was a Ku Klux Klan member.

May 17, 2021

Add me to the project mailing list.: Checked

**Response 06/19/23**

Dear John Whitcomb:

Thank you for connecting with El Paso County through our Briargate-Stapleton Project for Mobility website, by using our interactive map, and/or via email to share your roadway safety concerns and improvement preferences, including your reference to the Denver City Council rebranding of "Stapleton." Your input, which has been recorded for review, plays a valuable role in shaping the conceptual design of the roadway, designed to improve access and enhance safety for the entire community and provide long-term benefits for El Paso County.

The project name, Briargate Parkway/Stapleton Road Corridor Study, reflects the names of existing roadway segments within the project limits. The intent is to name the planned roadway extension between Black Forest Road and Meridian Road, "Briargate Parkway", consistent with the naming of the existing roadway west of Black Forest Road.

Although we are committed to ensuring equity in our transportation system and projects, we are unable to address "rebranding" of existing roadway segments that are outside the project limits through this project.

As requested, you have been added to the project mailing list.

Since the 30-day public comment period closed in September 2022, the project encountered some delay, but is now ready to progress in earnest. It is anticipated that both the Corridor Preservation Plan and the Access Control Plan will be presented to the Planning Commission and the Board of County Commissioners in mid to late summer for review and adoption, respectively. Presentation to the Planning Commission will occur at two regular meetings, the first as a public hearing. The County will advertise the specific dates of these meetings and meeting notices will also be posted on the project website.

Stay up to date by accessing our comprehensive website: <https://www.briargate-stapleton.com/>.

We appreciate your continued participation!

Maureen Paz de Araujo, FAICP CTP CEP  
 Senior Transportation Planner

**Tracked: Y**

**File/Record Number: 38a**

**From:** Nancy Dyo <reply-to+266ec22b3be9@crm.wix.com>  
**Sent:** Thursday, April 14, 2022, 8:23 PM  
**To:** Paz de Araujo, Maureen A. <Maureen.PazdeAraujo@wilsonco.com>;  
briargatestapleton@gmail.com  
**Subject:** [briargate-stapleton] Contacts Form - new submission

Nancy Dyo just submitted your form: Contacts Form  
on [briargate-stapleton](#)

**Message Details:**

Name: Nancy Dyo

Email: [nancy.dyo@gmail.com](mailto:nancy.dyo@gmail.com)

Subject: Interested in Project Updates

Message: I would like to be on your mailing list. Thank you! Add me to the project mailing list.: Checked

Response 00/00/22 **N/A Duplicate. See response in record 36b.**

**Tracked: Y**

**File/Record Number: 38b**

**From:** Nancy Dyo <reply-to+0296c9972354@crm.wix.com>  
**Sent:** Monday, August 22, 2022, 2:15 PM  
**To:** Paz de Araujo, Maureen <Maureen.PazdeAraujo@wilsonco.com>;  
briargatestapleton@gmail.com  
**Subject:** [briargate-stapleton] Contacts Form - new submission

**Nancy Dyo** just submitted your form: Contacts Form  
on [briargate-stapleton](#)

**Message Details:**

Name: Nancy Dyo

Email: [nancy.dyo@gmail.com](mailto:nancy.dyo@gmail.com)

Subject: Project Mailing List

Message: Please send me updates on the project. Add me to the project mailing list.: Checked

**Response 06/19/23**

Dear Nancy Dyo:

Thank you for connecting with us through our Briargate-Stapleton Project for Mobility website, by using our interactive map, and/or via email. As requested, you have been added to the project mailing list.

Since the 30-day public comment period closed in September 2022, the project encountered some delay, but is now ready to progress in earnest. It is anticipated that both the Corridor Preservation Plan and the Access Control Plan will be presented to the Planning Commission and the Board of County Commissioners in mid to late summer for review and adoption, respectively. Presentation to the Planning Commission will occur at two regular meetings, the first as a public hearing. The County will advertise the specific dates of these meetings and meeting notices will also be posted on the project website.

Stay up to date by accessing our comprehensive website: <https://www.briargate-stapleton.com/>.

We appreciate your continued participation!

Maureen Paz de Araujo, FAICP CTP CEP

Senior Transportation Planner

**Tracked: Y**

**File/Record Number: 39a**

**From:** Cherylee <reply-to+9dc9e9308232@crm.wix.com>  
**Sent:** Monday, August 22, 2022, 5:20 PM  
**To:** Paz de Araujo, Maureen <Maureen.PazdeAraujo@wilsonco.com>;  
briargatestapleton@gmail.com  
**Subject:** [briargate-stapleton] Contacts Form - new submission

Cherylee just submitted your form: Contacts Form  
on [briargate-stapleton](#)

**Message Details:**

Name: Cherylee

Email: [cherylee\\_granquist@yahoo.com](mailto:cherylee_granquist@yahoo.com)

Subject: Open land

Message: Leave Open land alone. If it's rural don't change to commercial.

Add me to the project mailing list.: Unchecked

Response 00/00/22 **N/A Duplicate submitter. Same concern addressed in 39b.**

**Tracked: Y**



**File/Record Number: 39b**

**From:** Cherylee <reply-to+3ba6f7e82180@crm.wix.com>  
**Sent:** Monday, August 22, 2022, 5:22 PM  
**To:** Paz de Araujo, Maureen <Maureen.PazdeAraujo@wilsonco.com>;  
 briargatestapleton@gmail.com  
**Subject:** [briargate-stapleton] Contacts Form - new submission

**Cherylee** just submitted your form: Contacts Form  
 on [briargate-stapleton](#)

**Message Details:**

Name: Cherylee

Email: [cherylee\\_granquist@yahoo.com](mailto:cherylee_granquist@yahoo.com)

Subject: Rural land

Message: Don't change rural land to commercial. Open areas are what COS should be as it's been. To much growth

Add me to the project mailing list.: Unchecked

**Response 06/19/23**

Dear Cherylee:

Thank you for connecting with El Paso County through our Briargate-Stapleton Project for Mobility website, by using our interactive map, and/or via email to share your roadway safety concerns and improvement preferences, and your preference to keep rural land free of commercial development and concern that there is too much growth in Colorado Springs. Your input, which has been recorded for review, plays a valuable role in shaping the conceptual design of the roadway, designed to improve access and enhance safety for the entire community and provide long-term benefits for El Paso County.

The types of development that are permitted within the study corridor are controlled by zoning which is outside the scope of this study. The current zoning in the area stems from actions that were taken from 2016 to 2021. The current uses and approved development plans are consistent with the adopted zoning for El Paso County in the project area. There are several zoning classifications in the project area that include residential and commercial uses. The conceptual design and access control for the roadway corridor is consistent with currently adopted zoning. Please contact El Paso County *Planner of the Day* at 719-520-6944 for more information about study area zoning and permitted land uses.

Since the 30-day public comment period closed in September 2022, the project encountered some delay, but is now ready to progress in earnest. It is anticipated that both the Corridor Preservation Plan and the Access Control Plan will be presented to the Planning Commission and the Board of County Commissioners in mid to late summer for review and adoption, respectively. Presentation to the Planning Commission will occur at two regular meetings, the first as a public hearing. The County will advertise the specific dates of these meetings and meeting notices will also be posted on the project website.

Stay up to date by accessing our comprehensive website: <https://www.briargate-stapleton.com/>.

We appreciate your continued participation!

Maureen Paz de Araujo, FAICP CTP CEP  
 Senior Transportation Planner

**Tracked: Y**

**File/Record Number: 40**

**From:** Cortney <reply-to+d15e2a9c376c@crm.wix.com>  
**Sent:** Monday, August 22, 2022, 3:00 PM  
**To:** Paz de Araujo, Maureen <Maureen.PazdeAraujo@wilsonco.com>;  
briargatestapleton@gmail.com  
**Subject:** [briargate-stapleton] Contacts Form - new submission

**Cortney** just submitted your form: Contacts Form  
on [briargate-stapleton](#)

**Message Details:**

Name: Cortney

Email: [cortneylyn528@gmail.com](mailto:cortneylyn528@gmail.com)

Subject: -

Message: -

Add me to the project mailing list.: Checked

**Response 06/19/23**

Dear Cortney:

Thank you for connecting with us through our Briargate-Stapleton Project for Mobility website, by using our interactive map, and/or via email. As requested, you have been added to the project mailing list.

Since the 30-day public comment period closed in September 2022, the project encountered some delay, but is now ready to progress in earnest. It is anticipated that both the Corridor Preservation Plan and the Access Control Plan will be presented to the Planning Commission and the Board of County Commissioners in mid to late summer for review and adoption, respectively. Presentation to the Planning Commission will occur at two regular meetings, the first as a public hearing. The County will advertise the specific dates of these meetings and meeting notices will also be posted on the project website.

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We appreciate your continued participation!

Maureen Paz de Araujo, FAICP CTP CEP  
Senior Transportation Planner

**Tracked: Y**





**File/Record Number: 41**

**From:** Michael Parmelee <reply-to+f25b89405d1a@crm.wix.com>  
**Sent:** Monday, August 22, 2022, 6:25 PM  
**To:** Paz de Araujo, Maureen <Maureen.PazdeAraujo@wilsonco.com>;  
 briargatestapleton@gmail.com  
**Subject:** [briargate-stapleton] Contacts Form - new submission

**Michael Parmelee** just submitted your form: Contacts Form  
 on [briargate-stapleton](#)

**Message Details:**

Name: Michael Parmelee

Email: [michael.parmelee@yahoo.com](mailto:michael.parmelee@yahoo.com)

Subject: Comments on Briargate/Stapleton proposed plan Message: In reviewing your proposed plan, I have some comments regarding it as proposed. The access/ingress at both Prairie Dove Drive and Liberty Grove go west only when exiting those streets. This forces much more traffic south on Towner through a rural residential area than there already is. People from those other two streets can't go directly to Meridian so they will go west on Stapleton to Towner and south on Towner. This is not acceptable to funnel that much more volume down our residential street on Towner. I also have some concern regarding the watershed from the new highway down Towner. We already have an overflow of water coming down Towner from the housing developments to the north of us, literally washing out the drainage ditches in even small storms. If any of the runoff from the new project (because it will have more hard surfaces with less natural absorption into the ground below) increases the runoff down Towner even by a small amount, it will be very damaging. I also notice you don't have a right turn lane on Towner going north at the light at Stapleton and Towner. Is this intentional? Wont' that back up traffic into our space when they are waiting to turn at the light? Thank you for listening to me and including my concerns in your plans.

Add me to the project mailing list.: Checked

**Response 06/19/23**

Dear Michael Parmelee:

Thank you for connecting with El Paso County through our Briargate-Stapleton Project for Mobility website, by using our interactive map, and/or via email to share your roadway safety concerns and improvement preferences. Your input, which has been recorded for review, plays a valuable role in shaping the conceptual design of the roadway, designed to improve access and enhance safety for the entire community and provide long-term benefits for El Paso County.

Your specific concerns are addressed below:

- West-only access/ingress at both Prairie Dove Drive and Liberty Grove forces too much traffic south on Towner Avenue through a residential area: [The recommended right-in/right-out access at Prairie Dove Drive and Liberty Grove is proposed consistent with El Paso County Engineering Design Criteria that restricts full access to a Principal Arterial roadway one-half mile spacing. This criterion preserves the function of these roadways to serve regional traffic and reduces not local traffic through adjacent residential neighborhoods. Exceptions are made to these criteria to provide access to adjacent properties, typically commercial properties, on a case by case basis. Because there is a quarter mile spacing between Meridian Road and Liberty Grove and commercial development is proposed in this area, full access may be allowed as a variance at that location as a variance to the Access Control Plan.](#)
- Increased runoff down Towner Avenue due to project's hard surfaces with less natural absorption into the ground below on top of runoff that already exists from housing developments to the north will cause more damage: [Drainage analysis and design will be completed as part of future preliminary and final design for the project.](#)
- No planned right-turn lane heading north at the light at Stapleton Drive and Towner Avenue may back up traffic into neighborhoods: [Future preliminary and final design for the project will include additional analysis of traffic operations, including detailed analysis for the Stapleton Drive and Towner Avenue intersection. If warranted, a northbound right-turn lane will be included in the design.](#)

Since the 30-day public comment period closed in September 2022, the project encountered some delay, but is now ready to progress in earnest. It is anticipated that both the Corridor Preservation Plan and the Access Control Plan will be presented to the Planning Commission and the Board of County Commissioners in mid to late summer for review and adoption, respectively. Presentation to the Planning Commission will occur at two regular meetings, the first as a public

hearing. The County will advertise the specific dates of these meetings and meeting notices will also be posted on the project website.

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We appreciate your continued participation!

Maureen Paz de Araujo, FAICP CTP CEP  
Senior Transportation Planner

Tracked: Y

**File/Record Number: 42**

**From:** gemalip2@gmail.com <reply-to+65dbf0fe3b4d@crm.wix.com>

**Sent:** Monday, August 22, 2022, 8:32 PM

**To:** Paz de Araujo, Maureen <Maureen.PazdeAraujo@wilsonco.com>;  
briargatestapleton@gmail.com

**Subject:** [briargate-stapleton] Contacts Form - new submission

A site visitor just submitted your form: Contacts Form  
on [briargate-stapleton](#)

**Message Details:**

Name: -

Email: [gemalip2@gmail.com](mailto:gemalip2@gmail.com)

Subject: I do NOT want to see segment 5 west of Vollmer road turned into commercial property.

Message: I do NOT want to see segment 5 west of Vollmer road turned into commercial property.

Add me to the project mailing list.: Unchecked

**Response 06/19/23**

Dear Anonymous:

Thank you for connecting with El Paso County through our Briargate-Stapleton Project for Mobility website, by using our interactive map, and/or via email to share your roadway safety concerns and improvement preferences, including your preference to keep Segment 5 west of Vollmer Road free of commercial development. Your input, which has been recorded for review, plays a valuable role in shaping the conceptual design of the roadway, designed to improve access and enhance safety for the entire community and provide long-term benefits for El Paso County.

Please note that development plans, for the most part, were approved prior to this study, which seeks input on the conceptual design of and access control for the roadway. [The types of development that are permitted within the study corridor are controlled by zoning which is outside the scope of this study.](#) The current zoning in the area stems from actions that were taken from 2016 to 2021. Existing uses and approved development plan land uses are consistent with the adopted zoning for El Paso County in the project area. There are several zoning classifications in the project area that include residential and commercial uses. The conceptual design and access control for the roadway corridor is consistent with currently adopted zoning. Please contact El Paso County *Planner of the Day* at 719-520-6944 for more information about study area zoning and permitted land uses.

Since the 30-day public comment period closed in September 2022, the project encountered some delay, but is now ready to progress in earnest. It is anticipated that both the Corridor Preservation Plan and the Access Control Plan will be presented to the Planning Commission and the Board of County Commissioners in mid to late summer for review and adoption, respectively. Presentation to the Planning Commission will occur at two regular meetings, the first as a public hearing. The County will advertise the specific dates of these meetings and meeting notices will also be posted on the project website.

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We appreciate your continued participation!

Maureen Paz de Araujo, FAICP CTP CEP  
Senior Transportation Planner

**Tracked: Y**

**File/Record Number: 43**

**From:** Sonja <reply-to+6bf2f4b46fc4@crm.wix.com>  
**Sent:** Monday, August 22, 2022, 9:19 PM  
**To:** Paz de Araujo, Maureen <Maureen.PazdeAraujo@wilsonco.com>;  
 briargatestapleton@gmail.com  
**Subject:** [briargate-stapleton] Contacts Form - new submission

**Sonja** just submitted your form: Contacts Form  
 on [briargate-stapleton](#)

**Message Details:**

Name: Sonja

Email: [sonja.madera@hotmail.com](mailto:sonja.madera@hotmail.com) Subject: Re-zoning near Vollmer

Message: I do NOT support the re-zoning of section 5 along Vollmer Road to commercial. Have you even been to the neighborhood to see what is around it? Five acre residential lots fit in with what is already there. Also, as you may have heard, there is a serious lack of housing right now, whereas commercial establishments lack both staff and customers as the country heads into a recession. So does the area need more restaurants without servers, or does it need more housing? (A: housing.)

Add me to the project mailing list.: Unchecked

**Response 06/19/23**

Dear Sonja:

Thank you for connecting with El Paso County through our Briargate-Stapleton Project for Mobility website, by using our interactive map, and/or via email to share your roadway safety concerns and improvement preferences, including your preference to keep Segment 5 west of Vollmer Road zoned only for housing and free of commercial development. Your input, which has been recorded for review, plays a valuable role in shaping the conceptual design of the roadway, designed to improve access and enhance safety for the entire community and provide long-term benefits for El Paso County.

Please note that development plans, for the most part, were approved prior to this study, which seeks input on the design of the roadway. [The types of development that are permitted within the study corridor are controlled by zoning which is outside the scope of this study.](#) The current zoning in the area stems from actions that were taken from 2016 to 2021. Existing uses and approved development plan land uses are consistent with the adopted zoning for El Paso County in the project area. There are several zoning classifications in the project area that include residential and commercial uses. The conceptual design and access control for the roadway corridor is consistent with currently adopted zoning. Please contact El Paso County [Planner of the Day](#) at 719-520-6944 for more information about study area zoning and permitted land uses.

Since the 30-day public comment period closed in September 2022, the project encountered some delay, but is now ready to progress in earnest. It is anticipated that both the Corridor Preservation Plan and the Access Control Plan will be presented to the Planning Commission and the Board of County Commissioners in mid to late summer for review and adoption, respectively. Presentation to the Planning Commission will occur at two regular meetings, the first as a public hearing. The County will advertise the specific dates of these meetings and meeting notices will also be posted on the [project website](#).

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We appreciate your continued participation!

Maureen Paz de Araujo, FAICP CTP CEP  
 Senior Transportation Planner

**Tracked: Y**

**File/Record Number: 44**

**From:** Mayra Hernandez <reply-to+ccd4ddf9b82a@crm.wix.com>  
**Sent:** Tuesday, August 23, 2022, 8:20 AM  
**To:** Paz de Araujo, Maureen <Maureen.PazdeAraujo@wilsonco.com>;  
 briargatestapleton@gmail.com  
**Subject:** [briargate-stapleton] Contacts Form - new submission

**Mayra Hernandez** just submitted your form: Contacts Form  
 on [briargate-stapleton](#)

**Message Details:**

Name: Mayra Hernandez

Email: [mlalvarez1111@gmail.com](mailto:mlalvarez1111@gmail.com)

Subject: I just built the house in this development

Message: Hello, I just built a house on the highland park development and my house will be very close to this project, we were looking at the plans and don't like the idea of the stop light, it needs to be a round about instead of the stop light. Also, a noise control wall.

Add me to the project mailing list.: Checked

**Response 06/19/23**

Dear Mayra Hernandez:

Thank you for connecting with El Paso County through our Briargate-Stapleton Project for Mobility website, by using our interactive map, and/or via email to share your roadway safety concerns and improvement preferences, particularly your desire to replace a stoplight with a roundabout and the addition of a noise barrier to protect your new home in the Highland Park Development. Your input, which has been recorded for review, plays a valuable role in shaping the conceptual design of the roadway, designed to improve access and enhance safety for the entire community and provide long-term benefits for El Paso County.

Your specific concerns are addressed below:

- Replace stoplight with a roundabout: [As noted in both the Corridor Preservation Plan and the Access Control Plan, traffic control \(roundabout or signalized intersection\) will be determined as a part of future preliminary and final design for the project. The selection will consider control of adjacent full-access intersection as well as adjacent land uses.](#)
- Install a noise barrier to buffer traffic noise from Highland Park residents close to the project area: [A noise study will be conducted to support environmental clearances and preliminary and final design for the project. The study will determine whether noise sensitive receivers \(residences, outdoor active use areas, etc.\) will be impacted by noise because of the proposed project. If noise impacts are identified, then noise wall mitigation will be evaluated to determine whether it would be reasonable \(cost versus receivers impacted\), feasible \(are many openings in the noise wall required to provide property access - through which noise could travel? Are there receivers located high above the roadway so that an excessively tall wall would be required?\), and effective \(would noise wall mitigation achieve a noise reduction of 5dBA or more?\).](#)

As requested, you have been added to the project mailing list.

Since the 30-day public comment period closed in September 2022, the project encountered some delay, but is now ready to progress in earnest. It is anticipated that both the Corridor Preservation Plan and the Access Control Plan will be presented to the Planning Commission and the Board of County Commissioners in mid to late summer for review and adoption, respectively. Presentation to the Planning Commission will occur at two regular meetings, the first as a public hearing. The County will advertise the specific dates of these meetings and meeting notices will also be posted on the project website.

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Maureen Paz de Araujo, FAICP CTP CEP  
 Senior Transportation Planner

**Tracked: Y**